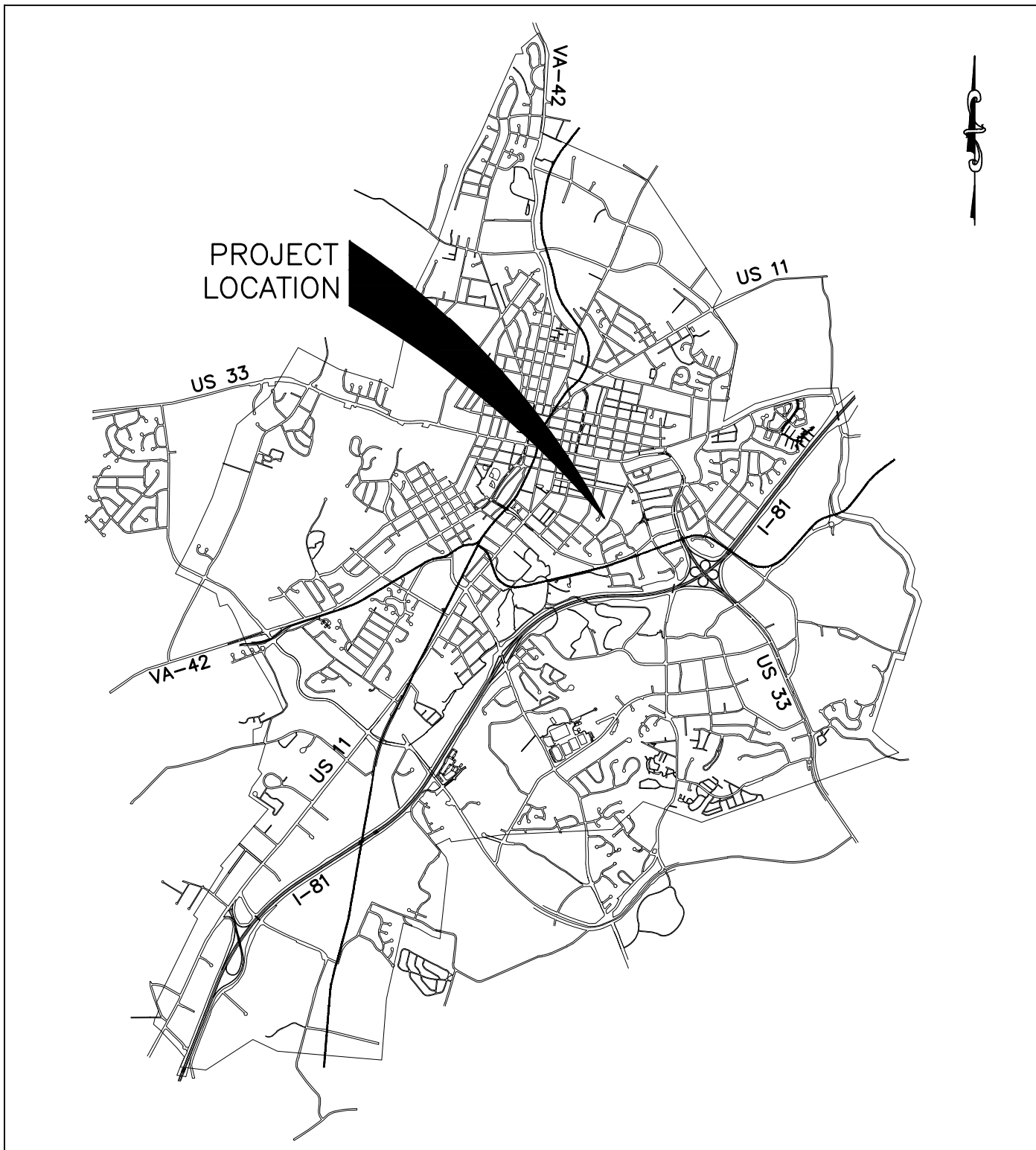


U:\Engineering Projects Comm Dev\Spotswood Elementary\H-Dwgs\PS-SHEETS\1-Title Notes.dwg Plotted: 2/11/2021 By: Matthew Huston

City of Harrisonburg

Mountain View Drive Trail



Vicinity Map

Scale: N.T.S.

Project Description

This project is located on a paper street right-of-way connecting Monticello Avenue and Myers Avenue, near Spotswood Elementary School. It consists of constructing approximately 340 LF of 5-ft wide gravel trail, as well as 25 LF of concrete sidewalk and two curb ramps. Re-grading and landscape reinstatement will be required. The disturbed area is estimated to be 6,830 SF.

60% Design
February 2021

Sheet Index

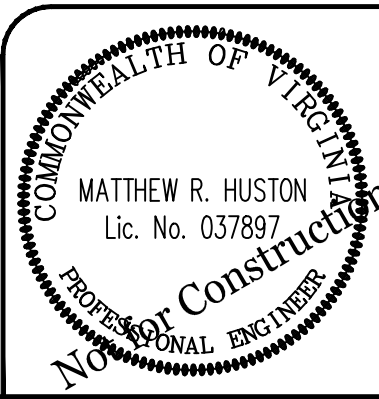
1	Cover Sheet
2	Legend & Abbreviations
3	General Notes and Erosion & Sediment Control Notes
4	Geometric Control
5	Existing Conditions
6	Typical Sections
7	Civil Details
8	Plan & Profile Sta 0+00 to 2+00
9	Plan & Profile Sta 2+00 to 4+00
10	Curb Ramp Details
11	Cross Sections 1
12	Cross Sections 2
13	Cross Sections 3
14	Cross Sections 4
--	Right-of-Way Exhibit

Project Contacts

Project Manager	Erin Yancey	(540) 434-5928
City Engineer	Dan Rublee	(540) 432-7700
Public Works	Tom Hartman	(540) 434-5928
Public Utilities	David Gray	(540) 434-9959
Harrisonburg Electric Commission	Zach Nyce	(540) 434-5361
Verizon	Chris Click	(540) 433-7676
Columbia Gas	Chris McDonald	(540) 851-2332
Comcast XFINITY	Philip Perry	(888) 266-2278



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Revisions				
Rev.	Date	Description	Init.	
1				

City of Harrisonburg
Engineering Division
409 S. Main St.
Harrisonburg, VA 22801



Mountain View Drive Trail Cover Sheet

Engineer:	MRH	Scale:	NO SCALE
Drawn By:	MRH	Date:	February 2021
Checked By:		Tax Map No:	026, 027

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of 14

LEGEND

- E - - - OH - - - EXISTING OVERHEAD ELECTRICAL LINE

— — — G — — — G - EXISTING GAS LINE

— — — W — — — W - EXISTING WATER LINE

— — — — — PROPERTY LINE

— — — — — EASEMENT LINE


———— LDA ————— LIMITS OF DISTURBED AREA

NEW CURB

NEW ADA CURB RAMP



NEW SIDEWALK


NEW GRAVEL TRAIL

 CONTROL POINT

WV
 EXISTING WATER VALVE

 EXISTING UTILITY POLE

—● EXISTING GUY WIRE

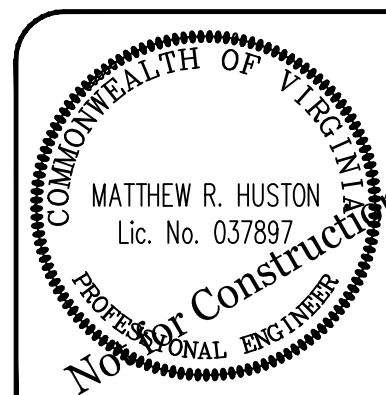
EXISTING SIGN

42.77 TBC FINISHED SPOT EL. W/ DESCRIPTION

ABBREVIATIONS

ASPH	ASPHALT
BK	BACK
BOW	BACK OF WALK
BP	BEGINNING POINT
BVCE	BEGINNING OF VERTICAL CURVE ELEVATION
BVCS	BEGINNING OF VERTICAL CURVE STATION
C&G	CURB AND GUTTER
CL	CLASS
COH	CITY OF HARRISONBURG
CONC	CONCRETE
EG	EXISTING GRADE
EL	ELEVATION
ELEV	ELEVATION
EOP	EDGE OF PAVEMENT
EP	ENDING POINT
EVCE	ENDING OF VERTICAL CURVE ELEVATION
EVCS	ENDING OF VERTICAL CURVE STATION
EX	EXISTING
FG	FINISHED GRADE
FT	FOOT
HEC	HARRISONBURG ELECTRIC COMMISSION
HORZ	HORIZONTAL
HYD	HYDRANT
L	LEFT
LF	LINEAR FEET
MAX	MAXIMUM
MIN	MINIMUM
O/S	OFFSET
PC	POINT OF CURVATURE
PCC	POINT OF COMPOUND CURVATURE
PI	POINT OF INTERSECTION
PROP	PROPOSED
PVI	POINT OF VERTICAL INTERSECTION
PVMT	PAVEMENT
PT	POINT OF TANGENCY
R	RIGHT
ROW	RIGHT-OF-WAY
R/W	RIGHT-OF-WAY
SF	SQUARE FEET
STA	STATION
SDWK	SIDEWALK
SY	SQUARE YARDS
TBC	TOP BACK OF CURB
TCE	TEMPORARY CONSTRUCTION EASEMENT
TRANS	TRANSITION
TY	TYPE
TYP	TYPICAL
VC	VERTICAL CURVE
VERT	VERTICAL

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City of Harrisonburg
Engineering Division
409 S. Main St.
Harrisonburg, VA 22801



Mountain View Drive Trail Legend & Abbreviations

Engineer:	MRH	Scale:	NO SCALE	Sheet Number: 2 of 14
Drawn By:	MRH	Date:	February 2021	
Checked By:		Tax Map No:	026, 027	

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CITY GENERAL NOTES

1. WORK FOR THIS PROJECT SHALL CONFORM TO THE LATEST EDITIONS OF THE VIRGINIA DEPARTMENT OF TRANSPORTATION (VDOT) ROAD AND BRIDGE SPECIFICATIONS, THE VDOT ROAD AND BRIDGE STANDARDS, THE VIRGINIA EROSION AND SEDIMENT CONTROL HANDBOOK, THE VIRGINIA EROSION AND SEDIMENT CONTROL REGULATIONS, AND THE CITY OF HARRISONBURG DESIGN AND CONSTRUCTION STANDARDS MANUAL. IN THE EVENT OF CONFLICT BETWEEN ANY OF THESE STANDARDS, SPECIFICATIONS OR PLANS, THE MOST STRINGENT SHALL GOVERN. ALL UTILITIES TO BE DEDICATED TO THE CITY OF HARRISONBURG MUNICIPAL WATER AND/OR SANITARY SEWER SYSTEM SHALL BE CONSTRUCTED AND TESTED TO CONFORM TO COMMONWEALTH OF VIRGINIA/STATE BOARD OF HEALTH WATERWORKS AND/OR SEWERAGE REGULATIONS AND THE CITY OF HARRISONBURG DESIGN AND CONSTRUCTION STANDARDS MANUAL.
2. EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED CONTINUOUSLY, RELOCATED AS NECESSARY AND SHALL BE CHECKED AFTER EVERY RUNOFF PRODUCING RAINFALL EVENT. SEEDED AREAS SHALL BE CHECKED REGULARLY AND SHALL BE WATERED, FERTILIZED, RESEEDED, AND MULCHED AS NECESSARY TO OBTAIN A DENSE STAND OF GRASS.
3. ALL DRAIN INLETS SHALL BE PROTECTED FROM SILTATION. INEFFECTIVE PROTECTION DEVICES SHALL BE IMMEDIATELY REPLACED AND THE INLET CLEANED. FLUSHING IS NOT AN ACCEPTABLE METHOD OF CLEANING.
4. THE LOCATION OF EXISTING UTILITIES AS SHOWN IS APPROXIMATE ONLY. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR LOCATING ALL PUBLIC AND PRIVATE UTILITIES WHICH LIE IN OR ADJACENT TO THE CONSTRUCTION SITE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING, AT ITS EXPENSE, ALL EXISTING UTILITIES DAMAGED DURING CONSTRUCTION. FORTY-EIGHT (48) HOURS PRIOR TO ANY EXCAVATION CALL "MISS UTILITY" AT (800) 552-7001.
5. INSTALLATION OF CONCRETE STORM PIPE SHALL COMPLY WITH VDOT STANDARD DRAWING PB-1.
6. ALL MATERIALS USED FOR FILL OR BACKFILL SHALL BE FREE OF WOOD, ROOTS, ROCKS, BOULDERS OR ANY OTHER NON-COMPACTIBLE SOIL TYPE MATERIAL. UNSATISFACTORY MATERIALS ALSO INCLUDE MAN-MADE FILLS AND REFUSE DEBRIS DERIVED FROM ANY SOURCE.
7. SATISFACTORY MATERIAL FOR USE AS FILL FOR PUBLIC STREETS INCLUDE MATERIAL CLASSIFIED IN ASTM D-2487 AS GW, GP, GM, GC, SW, SP, SM, SC, ML AND CL GROUPS. THE MOISTURE CONTENT SHALL BE CONTROLLED WITHIN PLUS OR MINUS 2 PERCENTAGE OF OPTIMUM MOISTURE CONTENT TO FACILITATE COMPACTION. GENERALLY, UNSATISFACTORY MATERIALS INCLUDE MATERIALS CLASSIFIED IN ASTM D-2487 AS PT, CH, MH, OL, OH AND ANY SOIL TOO WET TO FACILITATE COMPACTION. CH AND MH SOILS MAY BE USED SUBJECT TO APPROVAL OF THE CITY ENGINEER. SOILS SHALL HAVE A MINIMUM DRY DENSITY OF 92 LB./CU. FT. PER ASTM D-698 AND SHALL HAVE A PLASTICITY INDEX LESS THAN 12.
8. MATERIALS USED TO CONSTRUCT EMBANKMENTS FOR ANY PURPOSE, BACKFILL AROUND DRAINAGE STRUCTURES OR IN UTILITY TRENCHES OR ANY OTHER DEPRESSION REQUIRING FILL OR BACKFILL SHALL BE COMPACTED TO 95% OF MAXIMUM

- DENSITY. HAVE INSPECTOR INSPECT PRIOR TO ANY OPERATIONS INVOLVING FILLING OR BACKFILLING.
9. EMBANKMENT FILL SHALL BE PLACED IN LIFTS AT A MAXIMUM UNCOMPACTED DEPTH OF 8-INCHES, AND TRENCH BACKFILL SHALL BE PLACED IN LIFTS AT A MAXIMUM UNCOMPACTED DEPTH OF 6-INCHES.
10. ALL EXCAVATIONS, INCLUDING TRENCHES, SHALL BE KEPT DRY TO PROTECT THEIR INTEGRITY.
11. CITY INSPECTORS HAVE FULL AUTHORITY TO REJECT FILL OR BACKFILL MATERIALS, REQUIRE UNDERCUTTING OR SUBGRADE STABILIZATION, REQUIRE PROVISIONS FOR SUBDRAINAGE, OR REQUIRE OTHER MEASURES WHICH AFFECT THE INTEGRITY OF ROAD AND UTILITY CONSTRUCTION. FAILURE TO COMPLY WITH INSPECTOR'S DIRECTIVES SHALL BE CAUSE FOR NON-ACCEPTANCE TO THE FACILITY.
12. TRAFFIC CONTROL ON PUBLIC STREETS SHALL BE IN CONFORMANCE WITH THE VIRGINIA SUPPLEMENT TO THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND AS FURTHER DIRECTED BY CITY INSPECTORS.
13. ANY DISCREPANCIES FOUND BETWEEN THE DRAWINGS AND SPECIFICATIONS AND SITE CONDITIONS OR ANY INCONSISTENCIES OR AMBIGUITIES IN DRAWINGS OR SPECIFICATIONS SHALL BE IMMEDIATELY REPORTED TO THE ENGINEER, IN WRITING, WHO SHALL PROMPTLY ADDRESS SUCH INCONSISTENCIES OR AMBIGUITIES. WORK DONE BY THE CONTRACTOR AFTER HIS DISCOVERY OF SUCH DISCREPANCIES, INCONSISTENCIES, OR AMBIGUITIES SHALL BE DONE AT THE CONTRACTOR'S RISK.
14. A PRECONSTRUCTION CONFERENCE SHALL BE HELD PRIOR TO THE START OF THE CONSTRUCTION.

EROSION & SEDIMENT CONTROL NOTES

1. UNLESS OTHERWISE INDICATED, ALL VEGETATIVE AND STRUCTURAL EROSION AND SEDIMENT CONTROL PRACTICES SHALL BE CONSTRUCTED AND MAINTAINED ACCORDING TO THE MINIMUM STANDARDS AND SPECIFICATIONS OF THE VIRGINIA EROSION AND SEDIMENT CONTROL HANDBOOK AND VIRGINIA EROSION AND SEDIMENT CONTROL REGULATIONS.
2. SEDIMENT BASINS AND TRAPS, PERIMETER DIKES, SEDIMENT BARRIERS AND ANY OTHER MEASURES INTENDED TO TRAP SEDIMENT SHALL BE CONSTRUCTED AS A FIRST STEP IN ANY LAND DISTURBING ACTIVITY AND SHALL BE MADE FUNCTIONAL BEFORE UPSLOPE LAND DISTURBANCES TAKE PLACE.
3. A COPY OF THE APPROVED EROSION AND SEDIMENT CONTROL PLAN SHALL BE MAINTAINED ON THE SITE AT ALL TIMES.
4. THE CONTRACTOR IS RESPONSIBLE FOR THE INSTALLATION OF ANY ADDITIONAL EROSION CONTROL MEASURES NECESSARY TO PREVENT EROSION AND SEDIMENTATION AS DETERMINED BY THE PLAN APPROVING AUTHORITY.
5. ALL DISTURBED AREAS ARE TO DRAIN TO APPROVED SEDIMENT CONTROL MEASURES AT ALL TIMES DURING LAND DISTURBING ACTIVITIES AND DURING SITE DEVELOPMENT UNTIL FINAL STABILIZATION IS ACHIEVED.
6. DURING DEWATERING OPERATIONS, WATER SHALL BE PUMPED INTO AN APPROVED FILTERING DEVICE.
7. THE CONTRACTOR SHALL INSPECT ALL EROSION CONTROL MEASURES PERIODICALLY AND WITHIN 24 HOURS AFTER EACH RUNOFF PRODUCING RAINFALL EVENT. ANY NECESSARY REPAIRS OR CLEANUP TO MAINTAIN THE EFFECTIVENESS OF THE EROSION CONTROL DEVICES SHALL BE MADE IMMEDIATELY.
8. PERMANENT OR TEMPORARY SOIL STABILIZATION SHALL BE APPLIED TO DENUDED AREAS WITHIN SEVEN (7) DAYS AFTER FINAL GRADE IS REACHED ON ANY PORTION OF THE SITE. TEMPORARY SOIL STABILIZATION SHALL BE APPLIED WITHIN SEVEN (7) DAYS TO DENUDED AREAS THAT MAY NOT BE AT FINAL GRADE BUT WILL REMAIN DORMANT (UNDISTURBED FOR LONGER THAN 30 DAYS). PERMANENT STABILIZATION SHALL BE APPLIED TO AREAS THAT ARE TO BE LEFT DORMANT FOR MORE THAN ONE YEAR.
9. A PERMANENT VEGETATIVE COVER SHALL BE ESTABLISHED ON DENUDED AREAS NOT OTHERWISE PERMANENTLY STABILIZED. PERMANENT VEGETATION SHALL NOT BE CONSIDERED ESTABLISHED UNTIL A GROUND COVER IS ACHIEVED THAT IS UNIFORM, MATURE ENOUGH TO SURVIVE, AND WILL INHIBIT EROSION.
10. DURING CONSTRUCTION OF THE PROJECT, SOIL STOCK PILES SHALL BE STABILIZED OR PROTECTED WITH SEDIMENT TRAPPING MEASURES.
11. CUT AND FILL SLOPES SHALL BE CONSTRUCTED IN A MANNER THAT WILL MINIMIZE EROSION. SLOPES THAT ARE FOUND TO BE ERODING EXCESSIVELY WITHIN ONE YEAR OF PERMANENT STABILIZATION SHALL BE PROVIDED WITH ADDITIONAL SLOPE STABILIZING MEASURES UNTIL THE PROBLEM IS CORRECTED.

12. ALL STORM SEWER INLETS THAT ARE MADE OPERABLE DURING CONSTRUCTION SHALL BE PROTECTED SO THAT SEDIMENT-LADEN WATER CANNOT ENTER THE CONVEYANCE SYSTEM WITHOUT FIRST BEING FILTERED OR OTHERWISE TREATED TO REMOVE SEDIMENT.
13. UNDERGROUND UTILITY LINES SHALL BE INSTALLED IN ACCORDANCE WITH THE FOLLOWING STANDARDS IN ADDITION TO OTHER APPLICABLE CRITERIA:

a) NO MORE THAN 500 LINEAR FEET OF TRENCH MAY BE OPEN AT ANY GIVEN TIME.

b) EXCAVATED MATERIALS SHALL BE PLACED ON THE UPHILL SIDE OF TRENCHES.

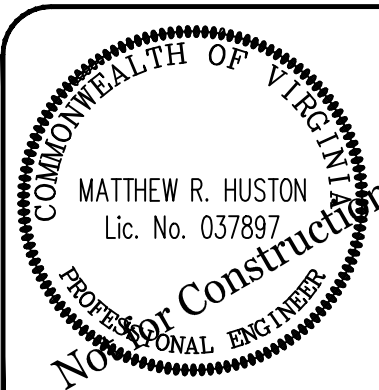
c) THE EFFLUENT FROM DEWATERING OPERATIONS SHALL BE FILTERED OR PASSED THROUGH AN APPROVED SEDIMENT TRAPPING DEVICE OR BOTH. THE EFFLUENT SHALL BE DISCHARGED IN A MANNER THAT DOES NOT ADVERSELY AFFECT FLOWING STREAMS OR OFFSITE PROPERTY.

d) MATERIAL USED FOR BACKFILLING TRENCHES SHALL BE PROPERLY COMPACTED IN ORDER TO MINIMIZE EROSION AND PROMOTE STABILIZATION.

e) RESTABILIZATION SHALL BE ACCOMPLISHED IN ACCORDANCE WITH THESE NOTES AND APPLICABLE REGULATIONS.

f) APPLICABLE SAFETY REGULATIONS SHALL BE COMPLIED WITH.
14. WHERE CONSTRUCTION VEHICLE ACCESS ROUTES INTERSECT PAVED ROAD OR PUBLIC ROADS, PROVISIONS SHALL BE MADE TO MINIMIZE THE TRANSPORT OF SEDIMENT BY VEHICULAR TRACKING ONTO THE PAVED SURFACE. WHERE SEDIMENT IS TRANSPORTED ONTO A PAVED OR PUBLIC ROAD SURFACE, THE ROAD SHALL BE CLEANED THOROUGHLY AT THE END OF EACH DAY. SEDIMENT SHALL BE REMOVED FROM THE ROAD BY SHOVELING OR SWEEPING AND TRANSPORTED TO A SEDIMENT CONTROL DISPOSAL AREA. STREET WASHING SHALL BE ALLOWED ONLY AFTER SEDIMENT IS REMOVED IN THIS MANNER.
15. ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN THIRTY (30) DAYS AFTER FINAL SITE STABILIZATION OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED, UNLESS OTHERWISE AUTHORIZED BY THE LOCAL PROGRAM ADMINISTRATOR. TRAPPED SEDIMENT AND THE DISTURBED SOIL AREAS RESULTING FROM THE DISPOSITION OF TEMPORARY MEASURES SHALL BE PERMANENTLY STABILIZED TO PREVENT FURTHER EROSION AND SEDIMENTATION.

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City of Harrisonburg
Engineering Division
409 S. Main St.
Harrisonburg, VA 22801



Mountain View Drive Trail General Notes			
Engineer:	MRH	Scale:	NO SCALE
Drawn By:	MRH	Date:	February 2021
Checked By:		Tax Map No:	026, 027
Sheet Number: 3			of 14

CONTROL POINT 4
N 6844622.407
E 11378737.971
EL 1455.760
#5 REBAR W/ YELLOW CAP

CONTROL POINT 3
N 6844597.716
E 11378881.295
EL 1453.850
#5 REBAR W/ YELLOW CAP

T.M. 26-S-15
LOT 5
JEFFERSON ACRES
AND A VACATED
PORTION OF A 20'
PUBLIC RIGHT OF WAY
GLENN H. GOETZ
& CAROL A. GOETZ
D.B. 1722, PG. 644

CONTROL POINT 2
N 6844558.580
E 11379019.003
EL 1443.670
#5 REBAR W/YELLOW CAP

T.M. 26-V-1
LOT 1, BLOCK A
PRESTON HEIGHTS
SUBDIVISION
SECTION 1
&
LOT 5A
JEFFERSON ACRES
CHRISTOPHER R. VERSEN
& SUSAN V. VERSEN
D.B. 4567, PG. 265

CONTROL POINT 1
N 6844482.969
E 11379135.482
EL 1434.950
#5 REBAR W/ YELLOW CAP

Field Engineering Notes

- The City will provide to the successful bidder sufficient field reference points, work sheets, and coordinate listings for the Contractor to be able to control the work. This provision is limited to the following:
 - Traverse points clearly marked and labeled in the field.
 - Copy of survey worksheet showing relative locations of traverse line, right-of-way, and easements.
 - Listing of coordinates for all traverse, easement and right-of-way centerline points and similar control points included on the above worksheet.
 - Vertical reference points located along project as shown on this sheet.
- The Contractor is responsible for layout of the work, using the above information for reference and initial control. When a licensed Land Surveyor is not regularly employed by the Contractor, or Subcontractors as the case may be, Contractor shall engage such person for establishing bench marks, important key elevations and all major lines of work.
- Contractor shall:
 - Furnish all labor and materials and perform all field engineering and construction layout necessary to ensure that the work conforms to the lines, grades, and elevations shown on the Plans
 - Locate and protect traverse and other control points before starting work on the site.
 - Preserve permanent reference points during progress of the Work.
 - Do not change or relocate reference points or items of the Work without specific approval from the Engineer.
 - Promptly advise the Engineer when a reference point is lost or destroyed, or requires relocation because of other changes in the Work.
 - Upon direction of the Engineer replace reference, points, stakes, or markers.
 - Locate such replacements according to the original survey control.

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Revisions				
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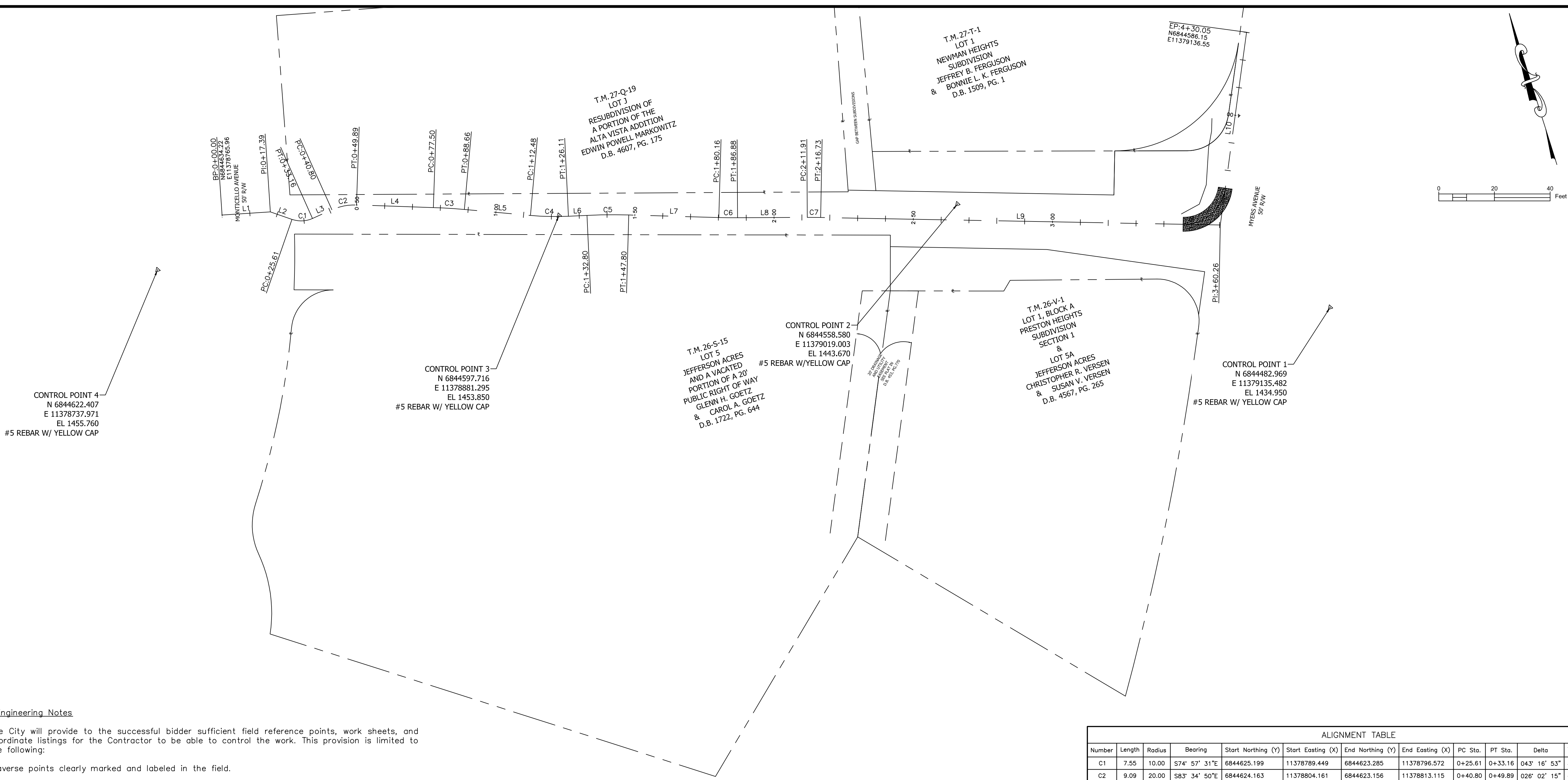
City of Harrisonburg
Engineering Division
409 S. Main St.
Harrisonburg, VA 22801



Mountain View Drive Trail
Geometric Control

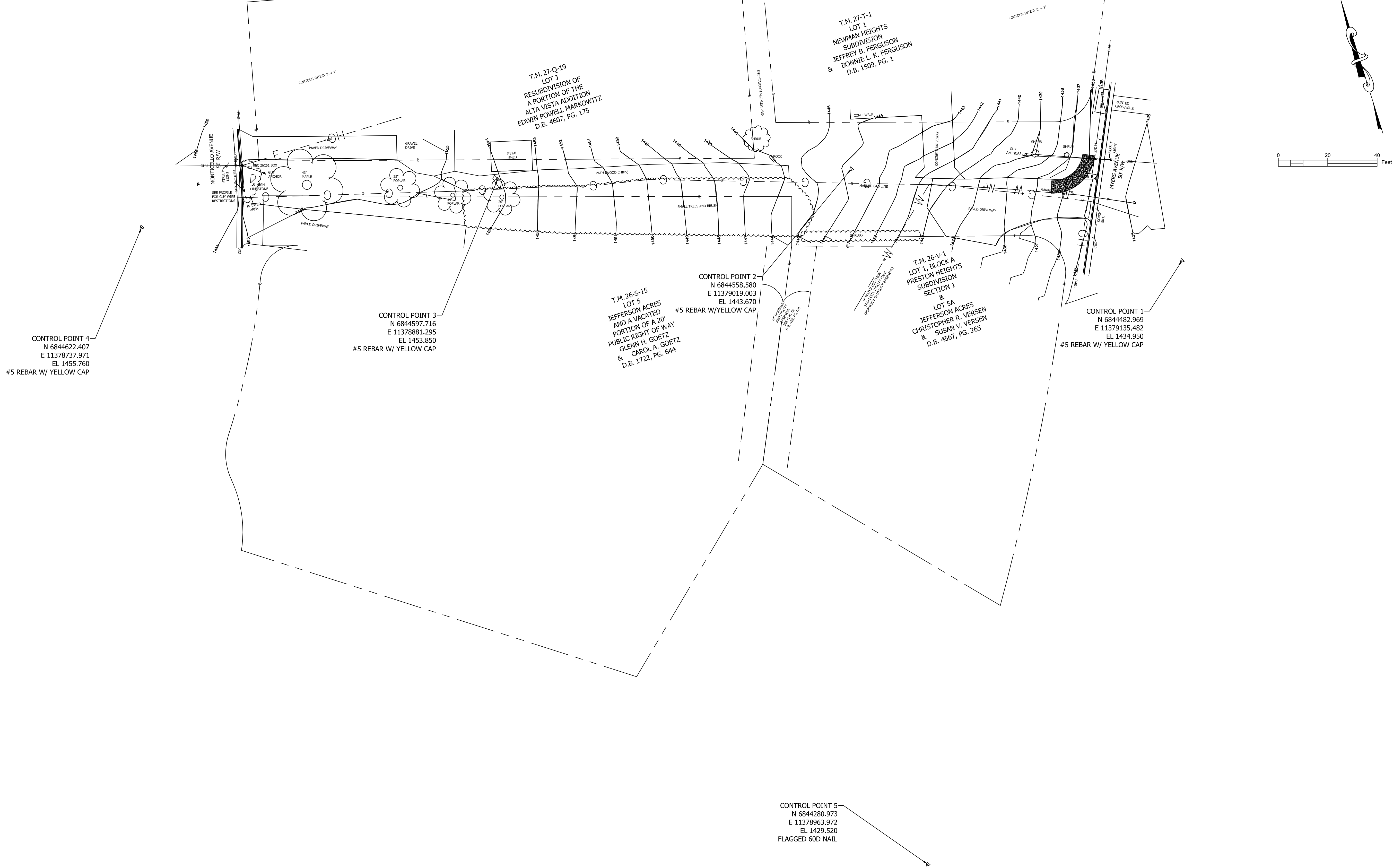
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Drawn By:	MRH	Date:	February 2021
Checked By:		Tax Map No:	026, 027

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ALIGNMENT TABLE												
Number	Length	Radius	Bearing	Start Northing (Y)	Start Easting (X)	End Northing (Y)	End Easting (X)	PC Sta.	PT Sta.	Delta	Tangent	PI Station
C1	7.55	10.00	S74° 57' 31"E	6844625.199	11378789.449	6844623.285	11378796.572	0+25.61	0+33.16	043° 16' 53"	3.97	0+29.58
C2	9.09	20.00	S83° 34' 50"E	6844624.163	11378804.161	6844623.156	11378813.115	0+40.80	0+49.89	026° 02' 15"	4.62	0+45.43
C3	11.16	200.00	S68° 57' 49"E	6844613.967	11378839.154	6844609.962	11378849.567	0+77.50	0+88.66	003° 11' 47"	5.58	0+83.08
C4	13.63	100.00	S71° 16' 13"E	6844600.795	11378871.552	6844596.421	11378884.451	1+12.48	1+26.11	007° 48' 36"	6.83	1+19.31
C5	15.00	200.00	S73° 01' 35"E	6844594.711	11378890.913	6844590.332	11378905.258	1+32.80	1+47.80	004° 17' 52"	7.50	1+40.30
C6	6.72	200.00	S71° 50' 24"E	6844579.732	11378935.831	6844577.638	11378942.215	1+80.16	1+86.88	001° 55' 30"	3.36	1+83.52
C7	4.82	200.00	S72° 06' 43"E	6844570.235	11378966.133	6844568.754	11378970.721	2+11.91	2+16.73	001° 22' 52"	2.41	2+14.32
L1	17.39		N76° 20' 31"W	6844634.215	11378765.960	6844630.109	11378782.858					
L2	8.22		S53° 19' 05"E	6844630.109	11378782.858	6844625.199	11378789.449					
L3	7.64		N83° 24' 03"E	6844623.285	11378796.572	6844624.163	11378804.161					
L4	27.61		S70° 33' 42"E	6844623.156	11378813.115	6844613.967	11378839.154					
L5	23.82		S67° 21' 55"E	6844609.962	11378849.567	6844600.795	11378871.552					
L6	6.68		S75° 10' 31"E	6844596.421	11378884.451	6844594.711	11378890.913					
L7	32.36		S70° 52' 39"E	6844590.332	11378905.258	6844579.732	11378935.831					
L8	25.04		S72° 48' 09"E	6844577.638	11378942.215	6844570.235	11378966.133					
L9	143.52		S71° 25' 17"E	6844568.754	11378970.721	6844523.027	11379106.764					
L10	69.80		N25° 15' 55"E	6844523.027	11379106.764	6844586.146	11379136.554					

CONTROL POINT 5
N 6844280.973
E 11378963.972
EL 1429.520
FLAGGED 60D NAIL



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Revisions				
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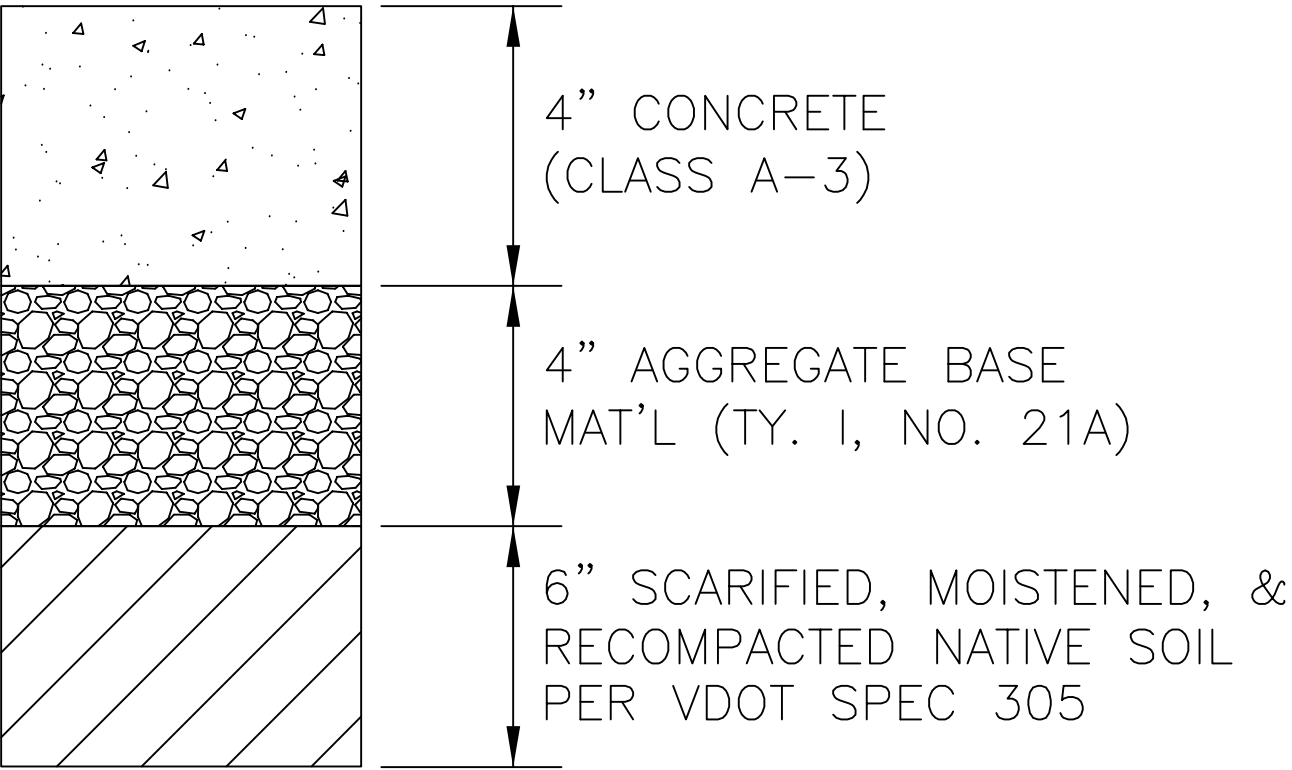
City of Harrisonburg
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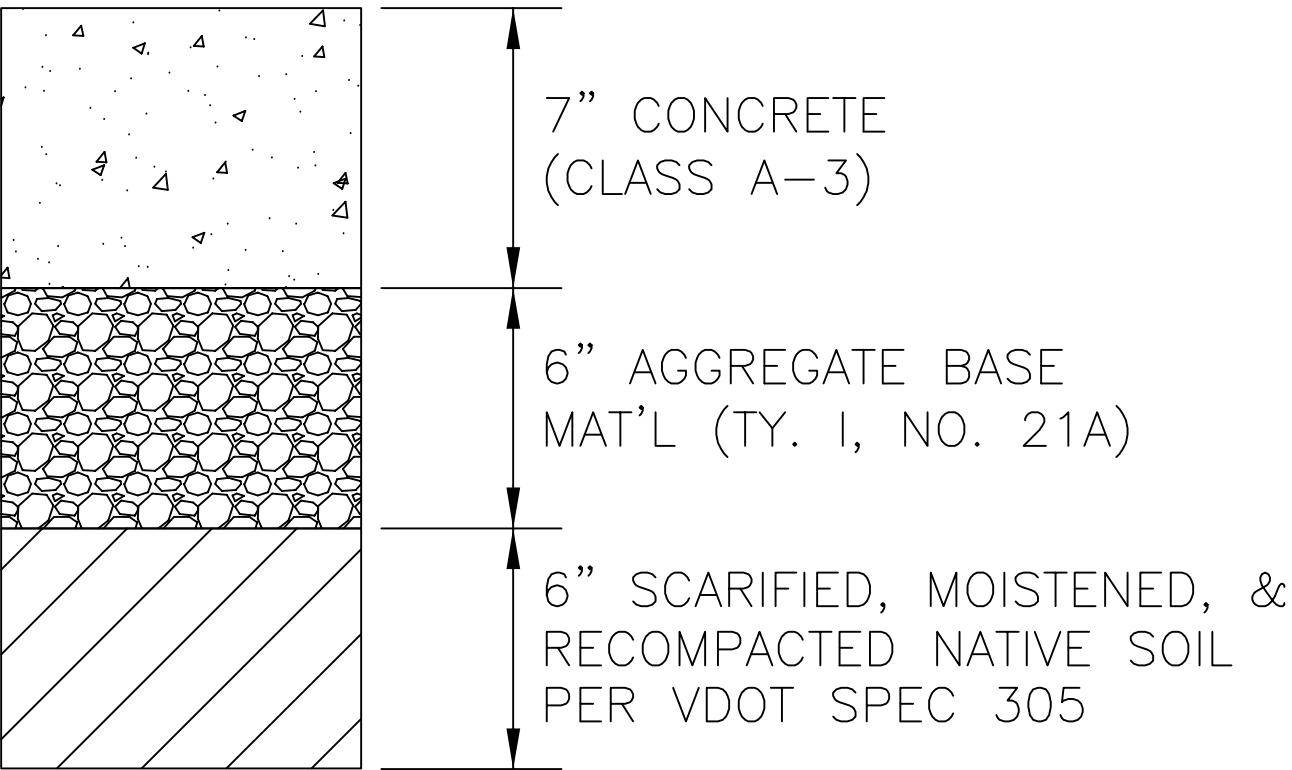
Mountain View Drive Trail Existing Conditions					
Engineer:	MRH	Scale:	1" = 20'		
Drawn By:	MRH	Date:	February 2021	Sheet Number:	5
Checked By:		Tax Map No:	026, 027	of	14

U:\Engineering Projects Comm Dev\Spotswood Elementary\H-Dwgs\PS-SHEETS\2-Geometrics (Concept 3).dwg Plotted: 2/11/2021 By: Matthew Huston

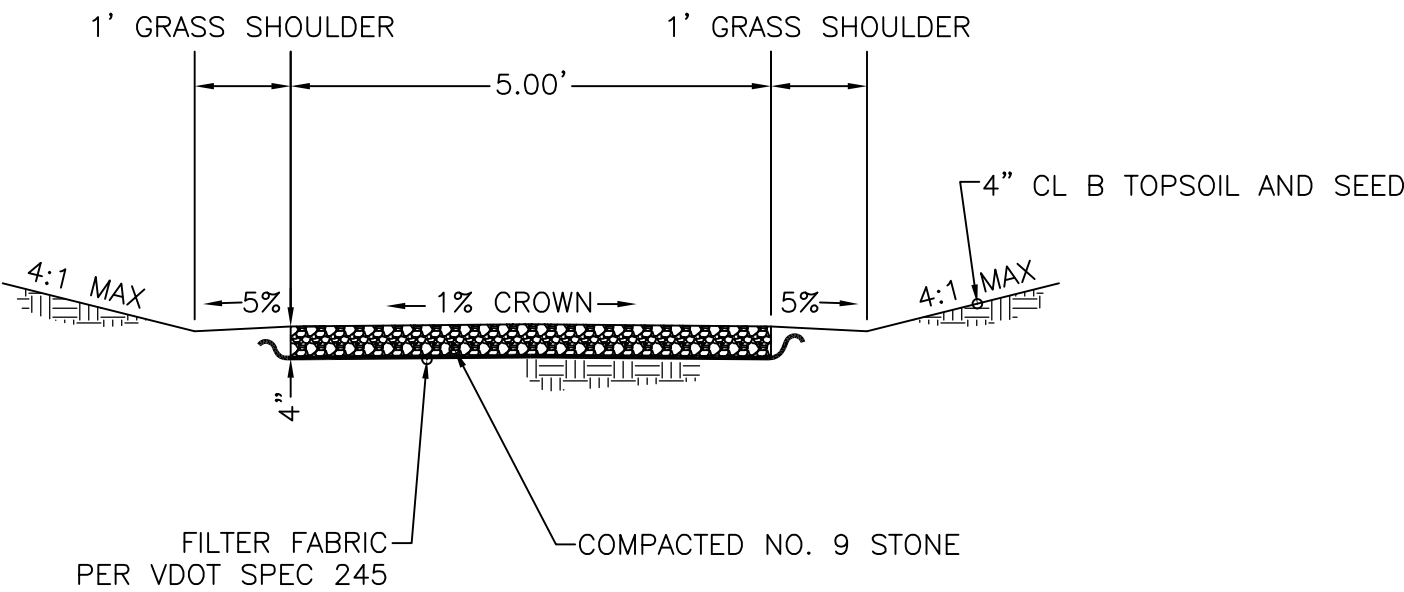
- NOTES:
- EXPANSION JOINTS SHALL BE INSTALLED EVERY 100 FT. AT ALL RADII POINTS AT CONCRETE ENTRANCES AND CURB RETURNS, AND AT LOCATIONS NO LESS THAN 6 FT AND NO MORE THAN 10 FT FROM DROP INLETS. EXPANSION MATERIAL SHALL BE ½” THICK AND SHALL EXTEND THE FULL DEPTH OF CONTACT SURFACE.
 - CONTRACTION JOINTS SHALL BE 1” DEEP BY ⅛” – ¼” WIDE AT INTERVALS EQUAL TO SIDEWALK WIDTH.
 - USE ¼” RADIUS ON ALL EDGES.
 - CURE FOR 72 HOURS USING CURING COMPOUND OR MOISTURE.
 - RECOMMENDED STONE GRADATION FOR GRAVEL TRAIL SURFACE:
 - 3/8” SIEVE – 100% PASSING
 - 1/4” SIEVE – 65% PASSING
 - 3/16” SIEVE – 50% PASSING
 - 1/8” SIEVE – 35% PASSING
 - 1/16” SIEVE – 25% PASSING



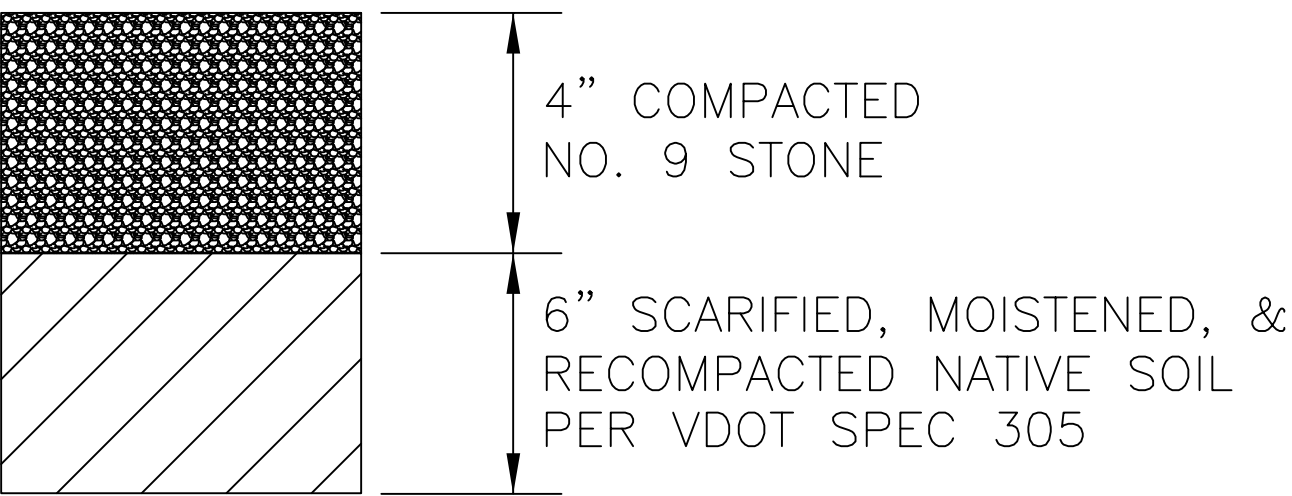
TYPICAL SIDEWALK PAVEMENT SECTION
SCALE: N.T.S.



TYPICAL CURB RAMP PAVEMENT SECTION
SCALE: N.T.S.

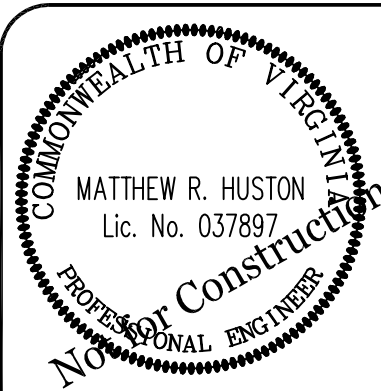


GRAVEL TRAIL SECTION



TYPICAL GRAVEL TRAIL SECTION
SCALE: N.T.S.

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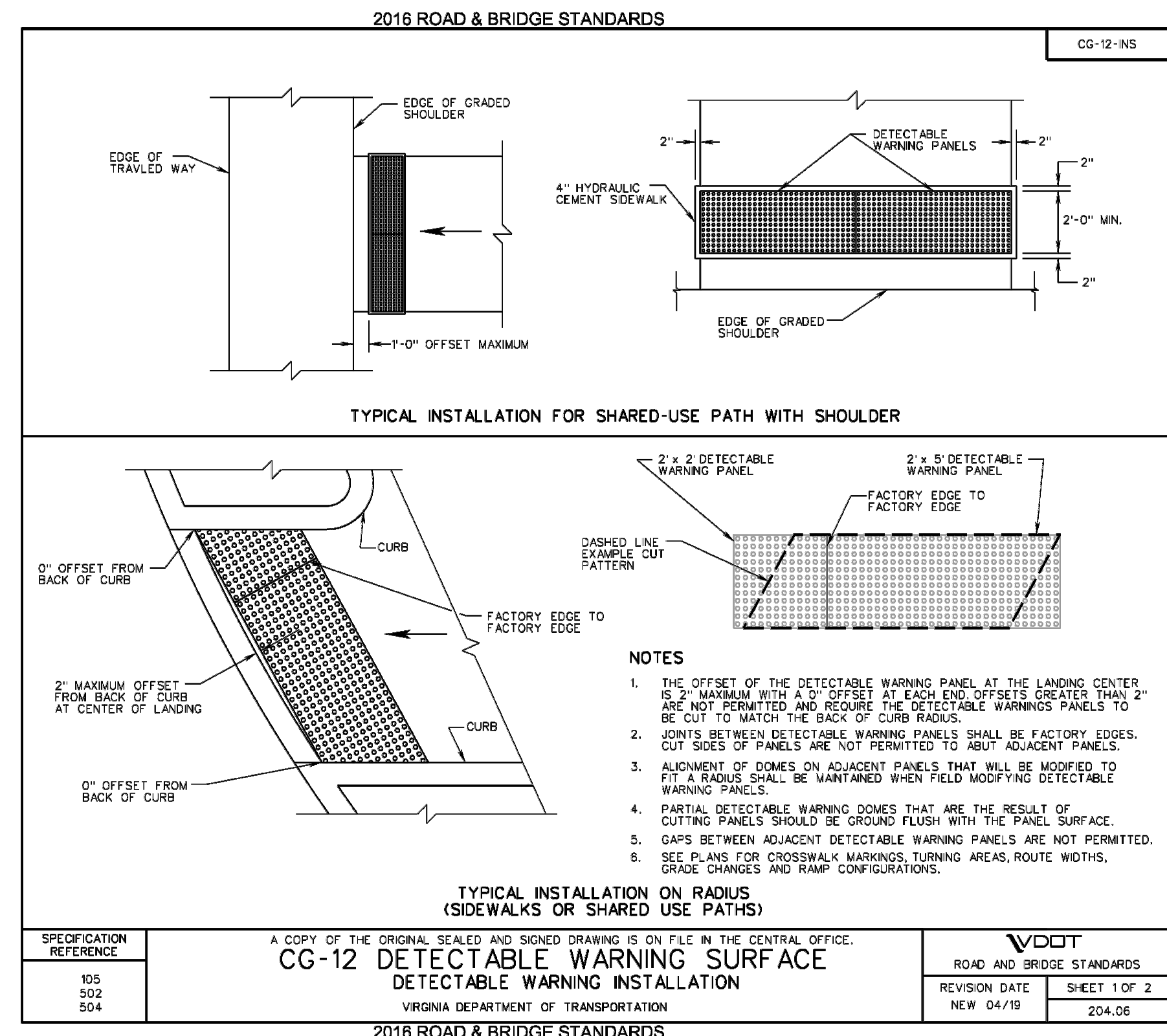
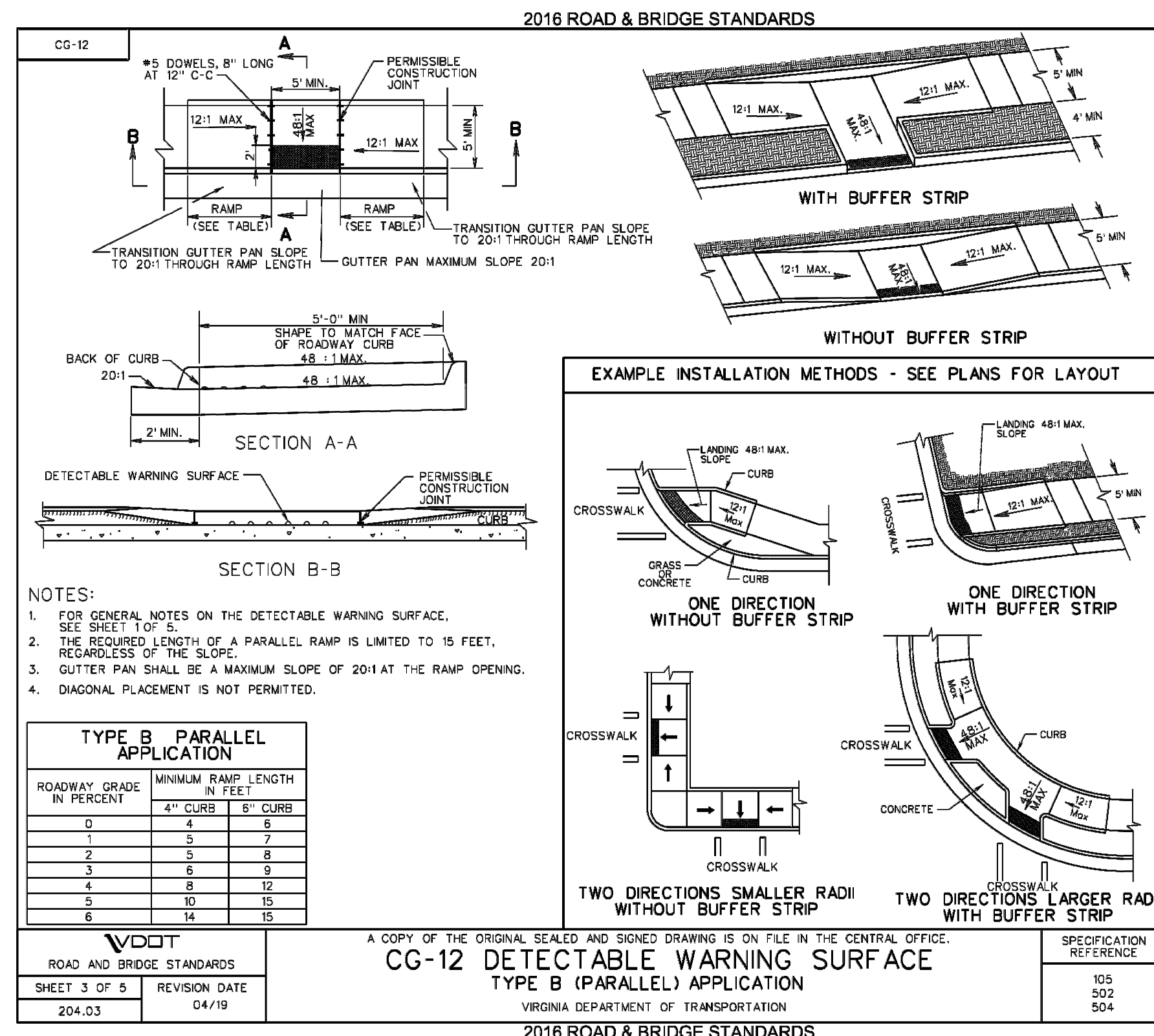
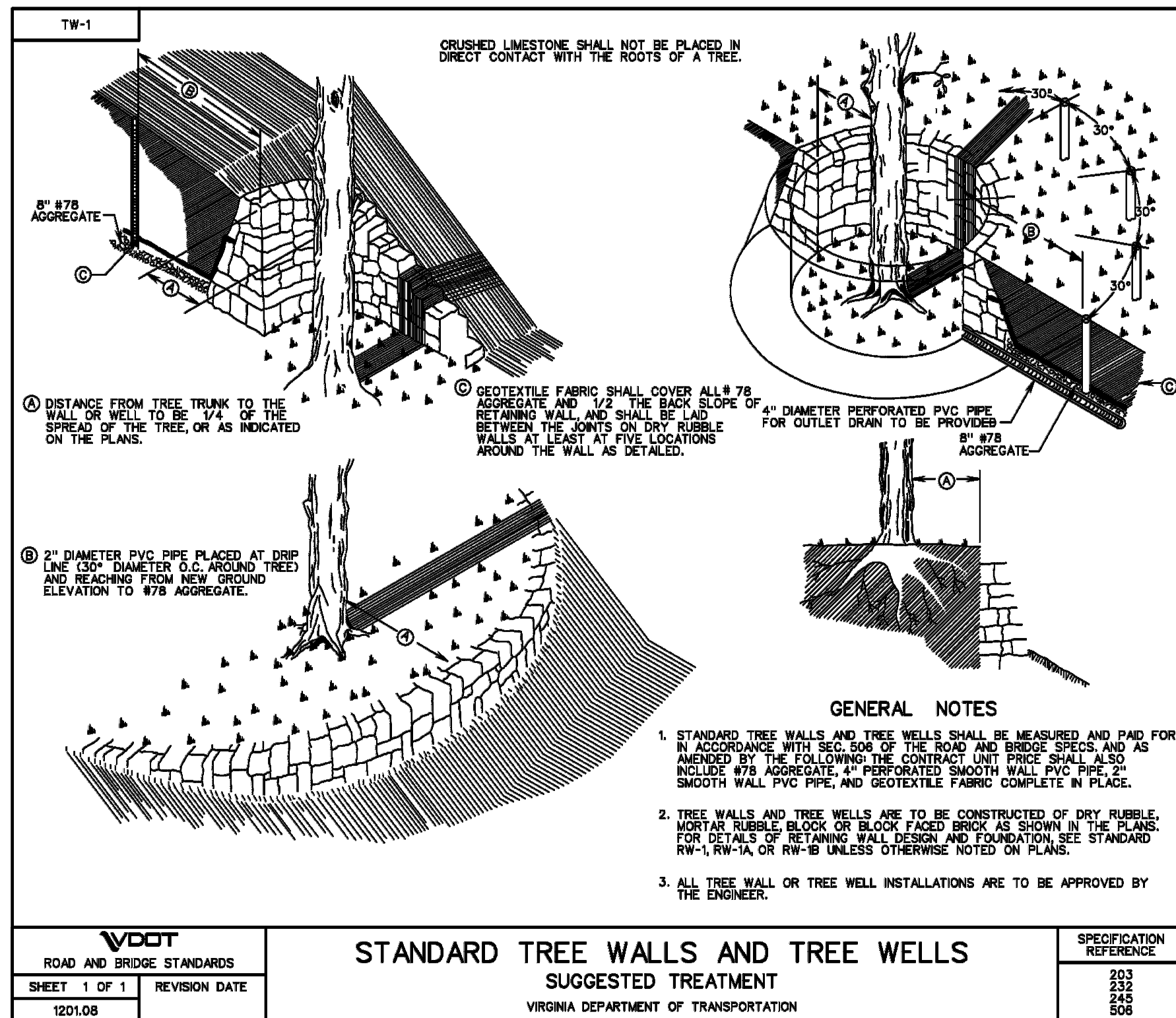


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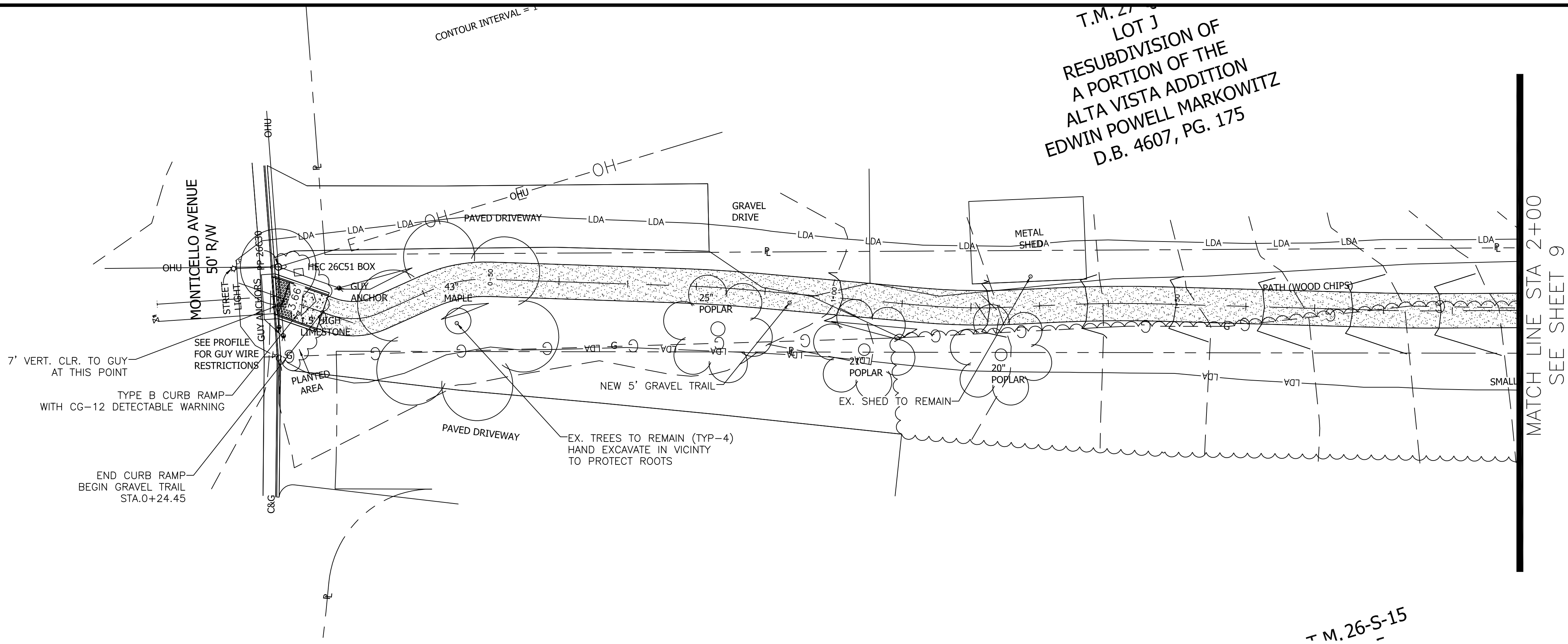
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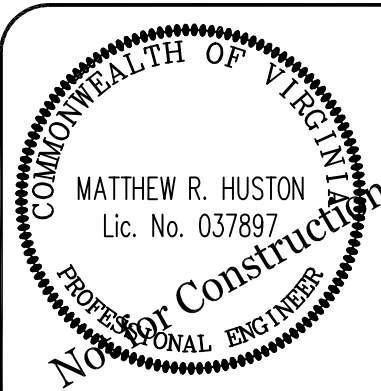
Mountain View Drive Trail Typical Sections					
Engineer:	MRH	Scale:	N.T.S.		
Drawn By:	MRH	Date:	February 2021	Sheet Number:	6
Checked By:		Tax Map No:	026, 027	of	14



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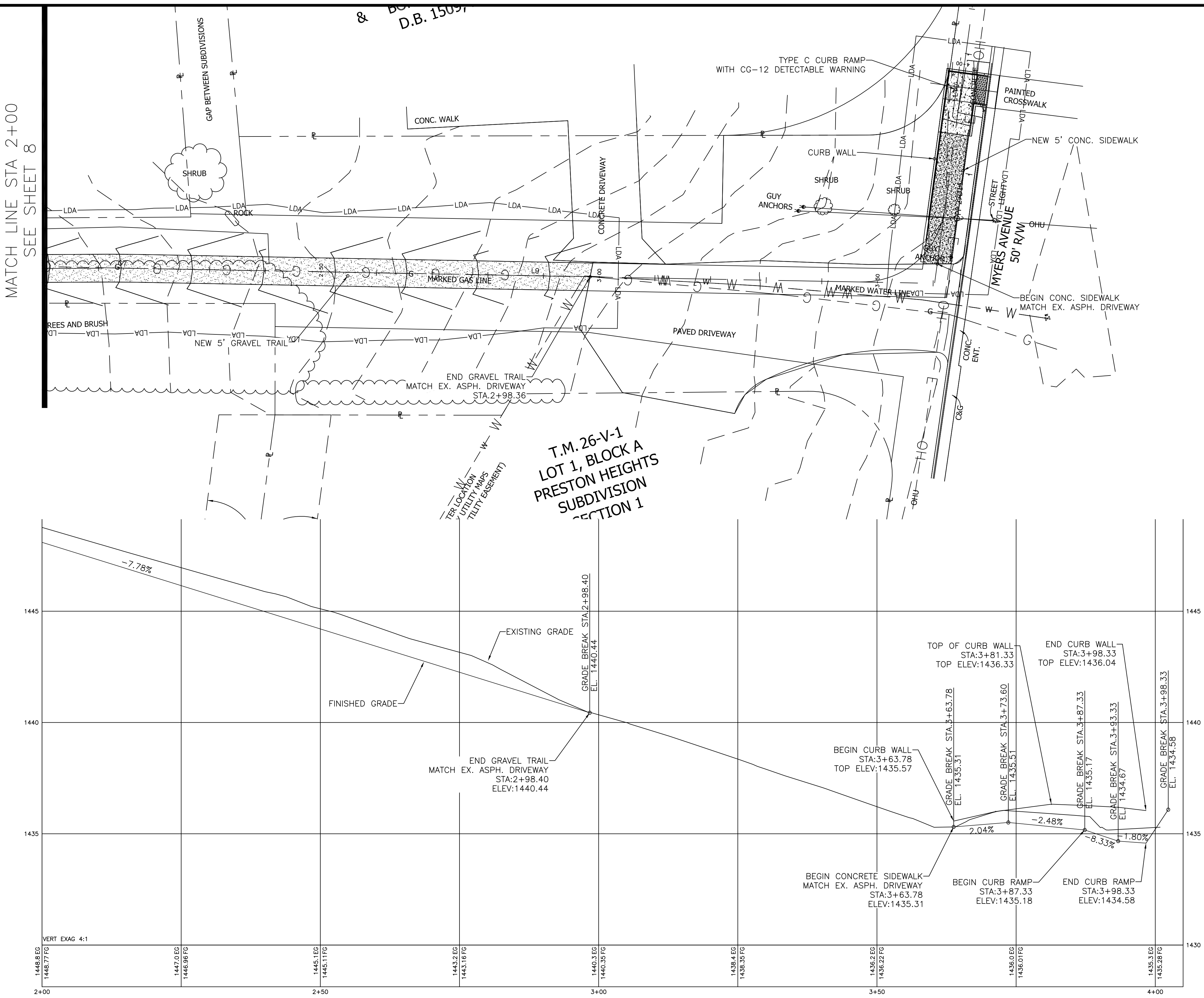
City of Harrisonburg
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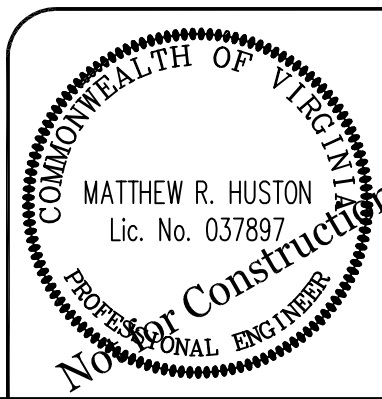
Mountain View Drive Trail
Plan and Profile Sta 0+00 to 2+00

Engineer:	MRH	Scale:	1" = 10'
Drawn By:	MRH	Date:	February 2021
Checked By:		Tax Map No:	026, 027

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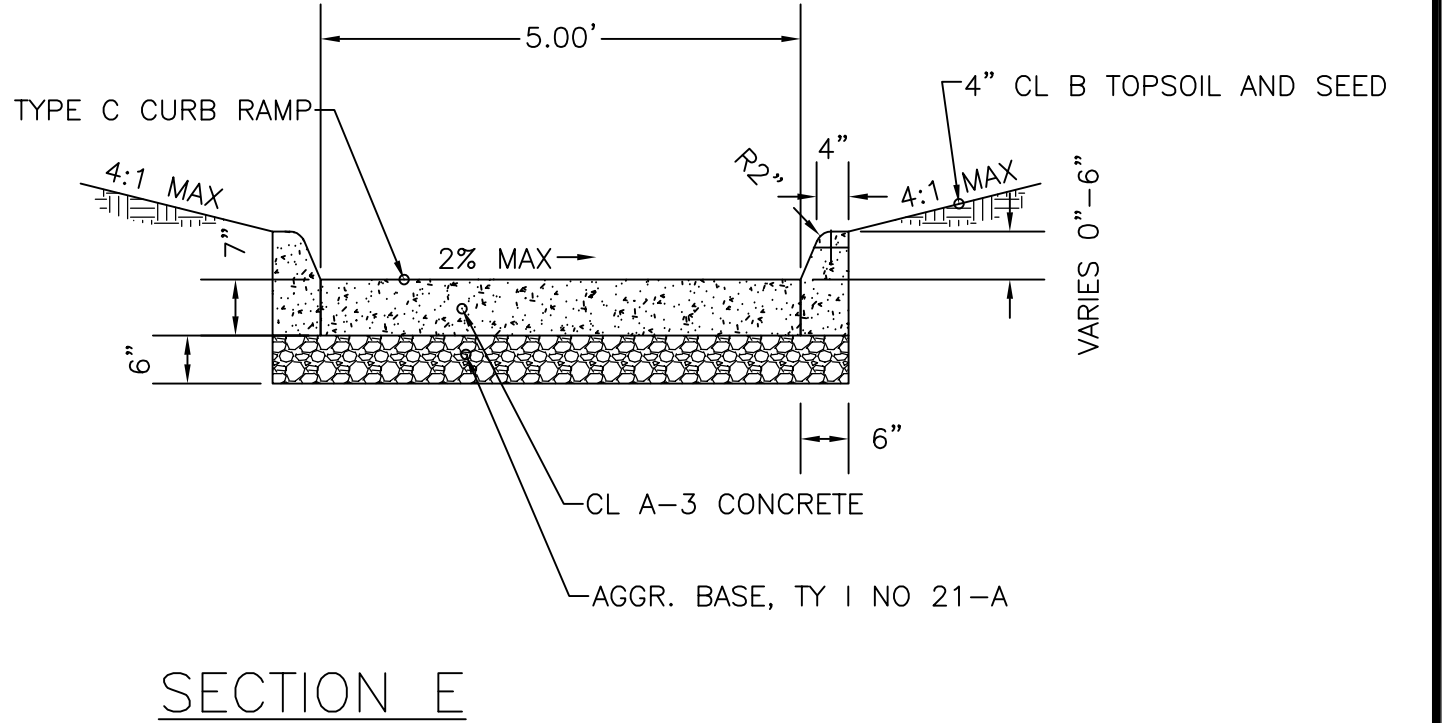
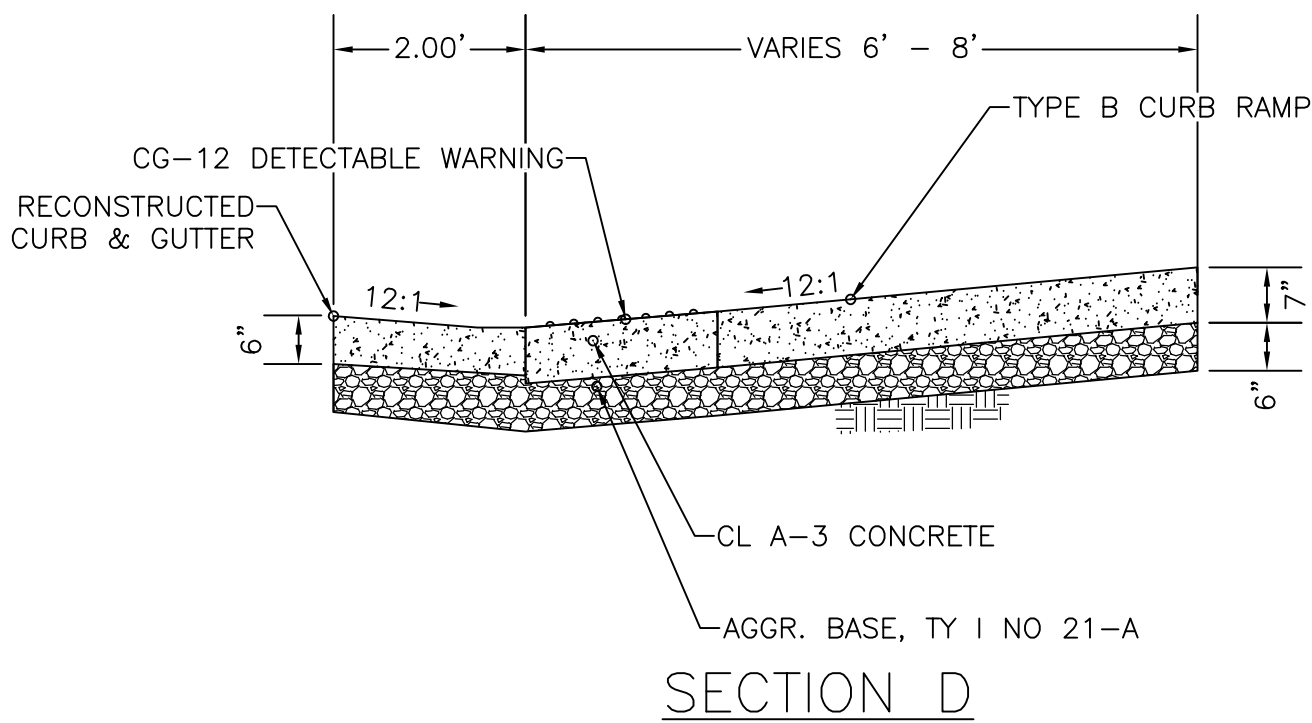
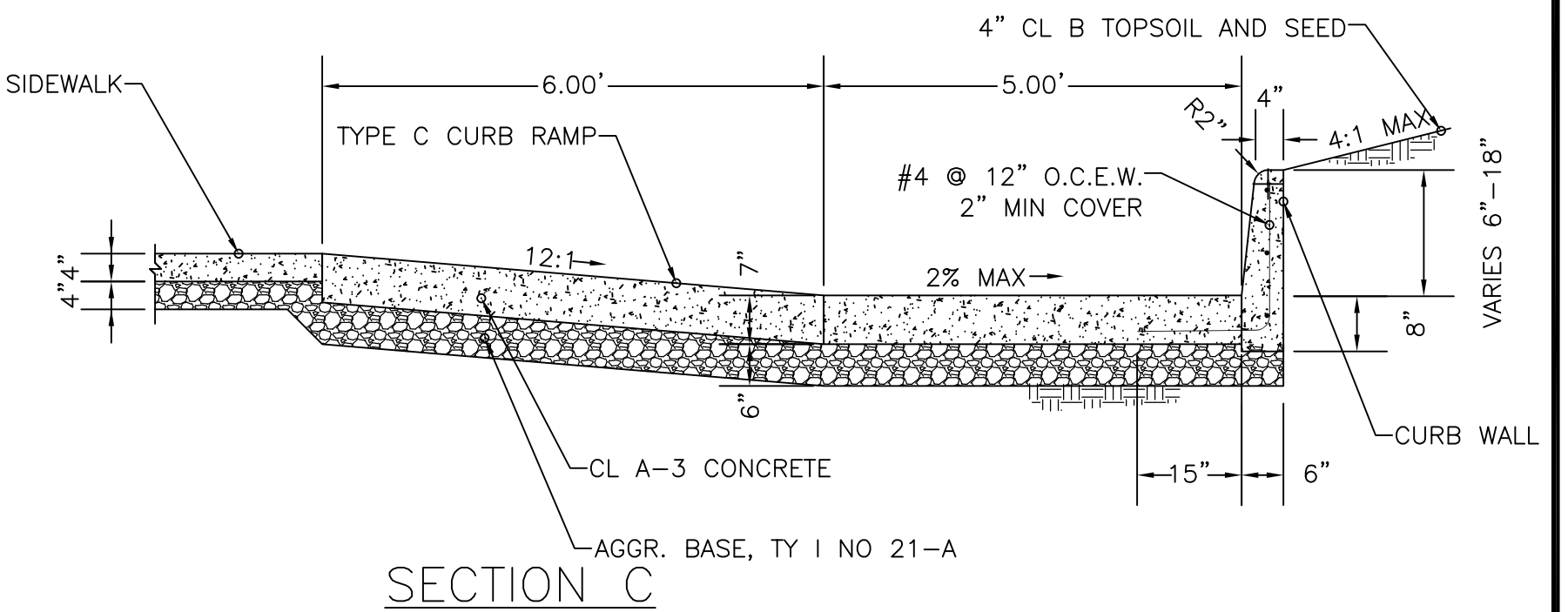
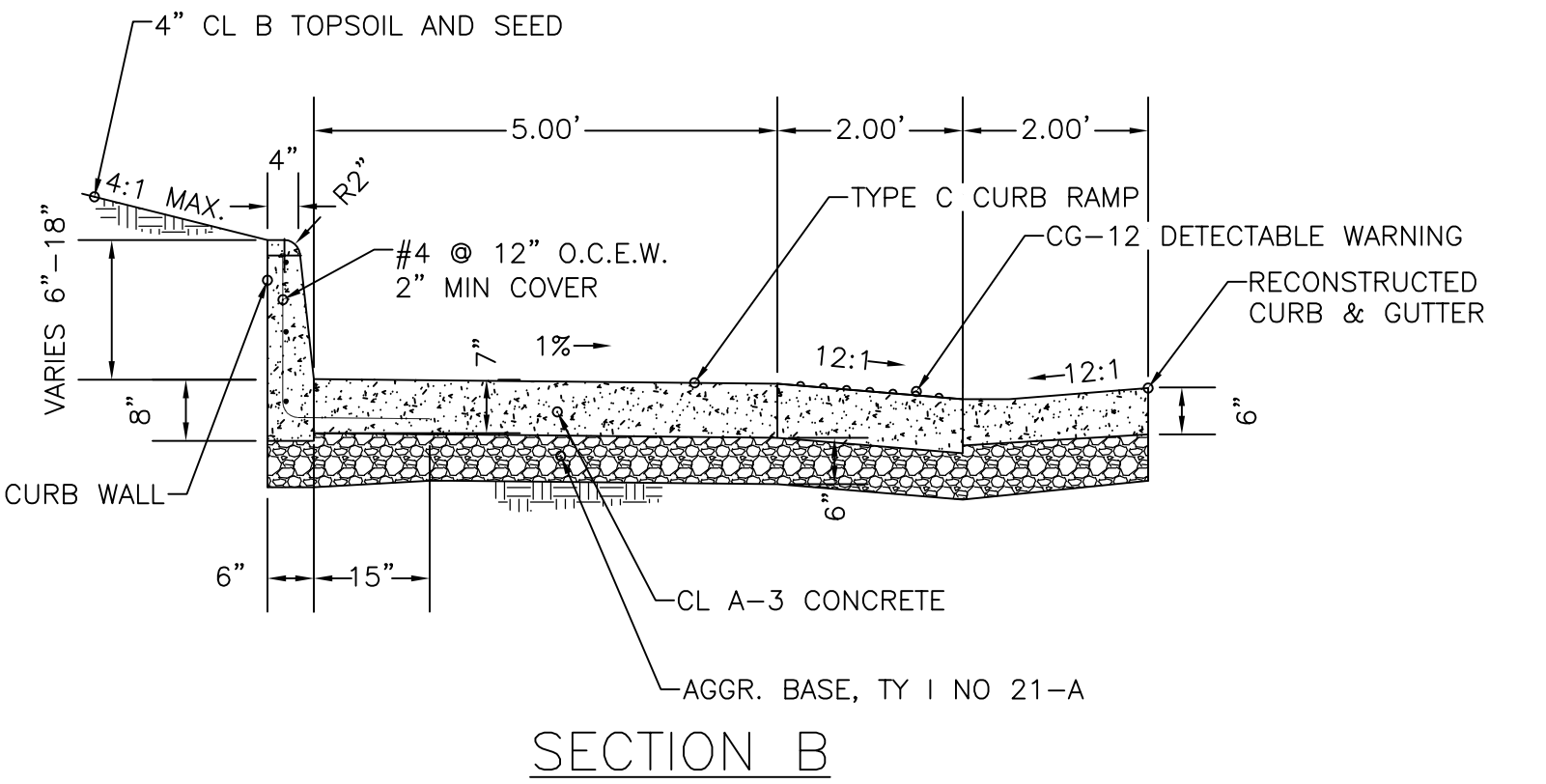
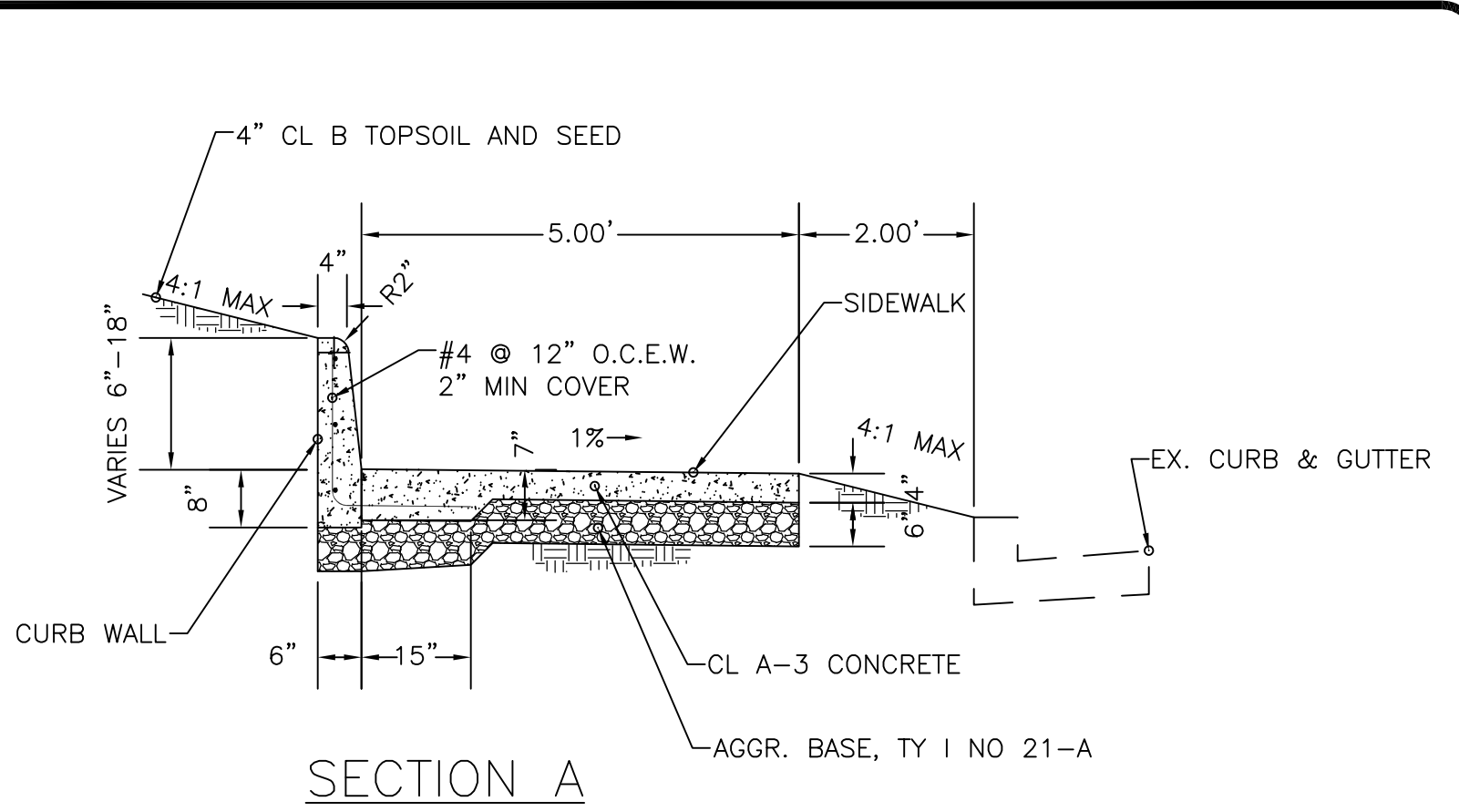
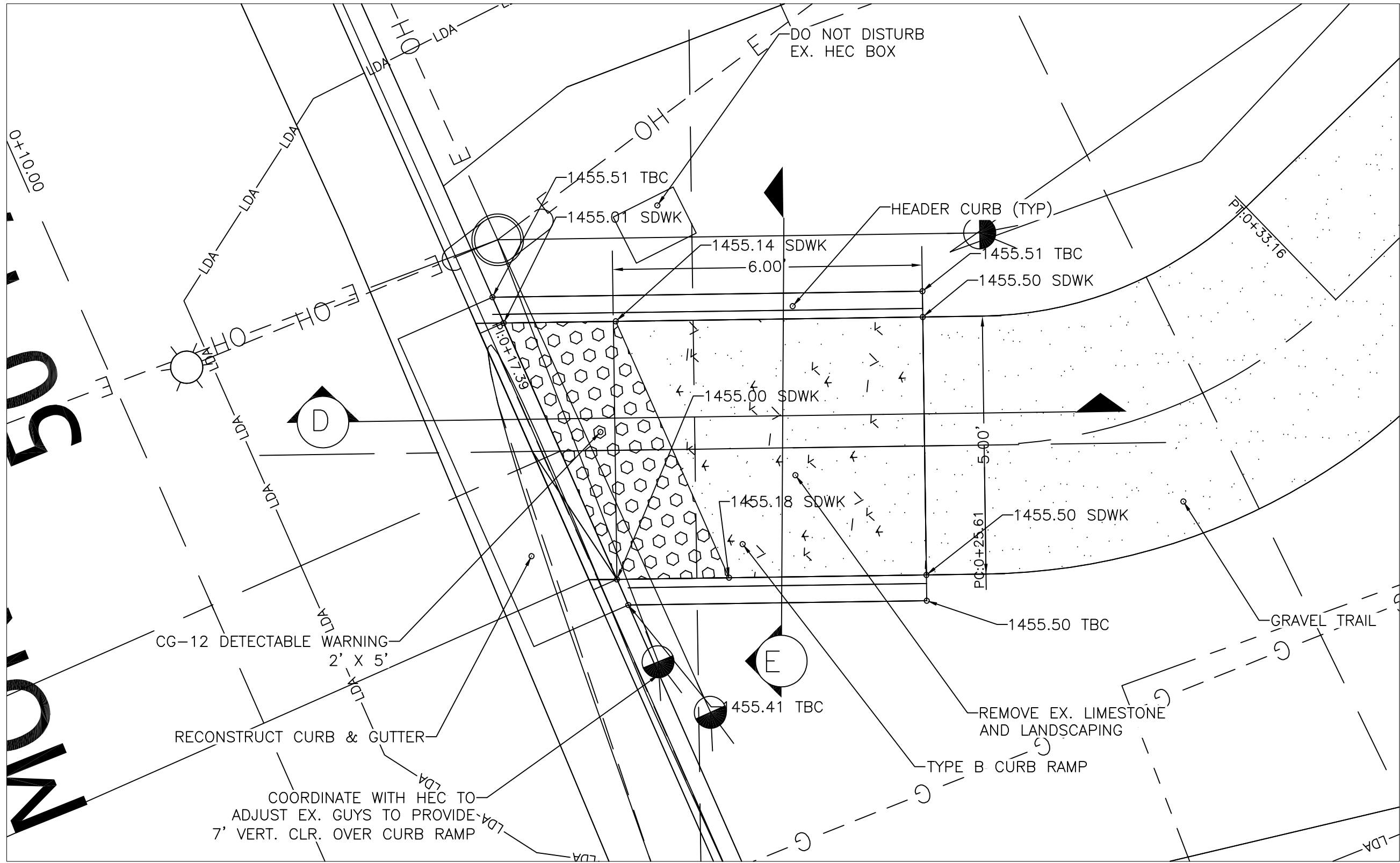
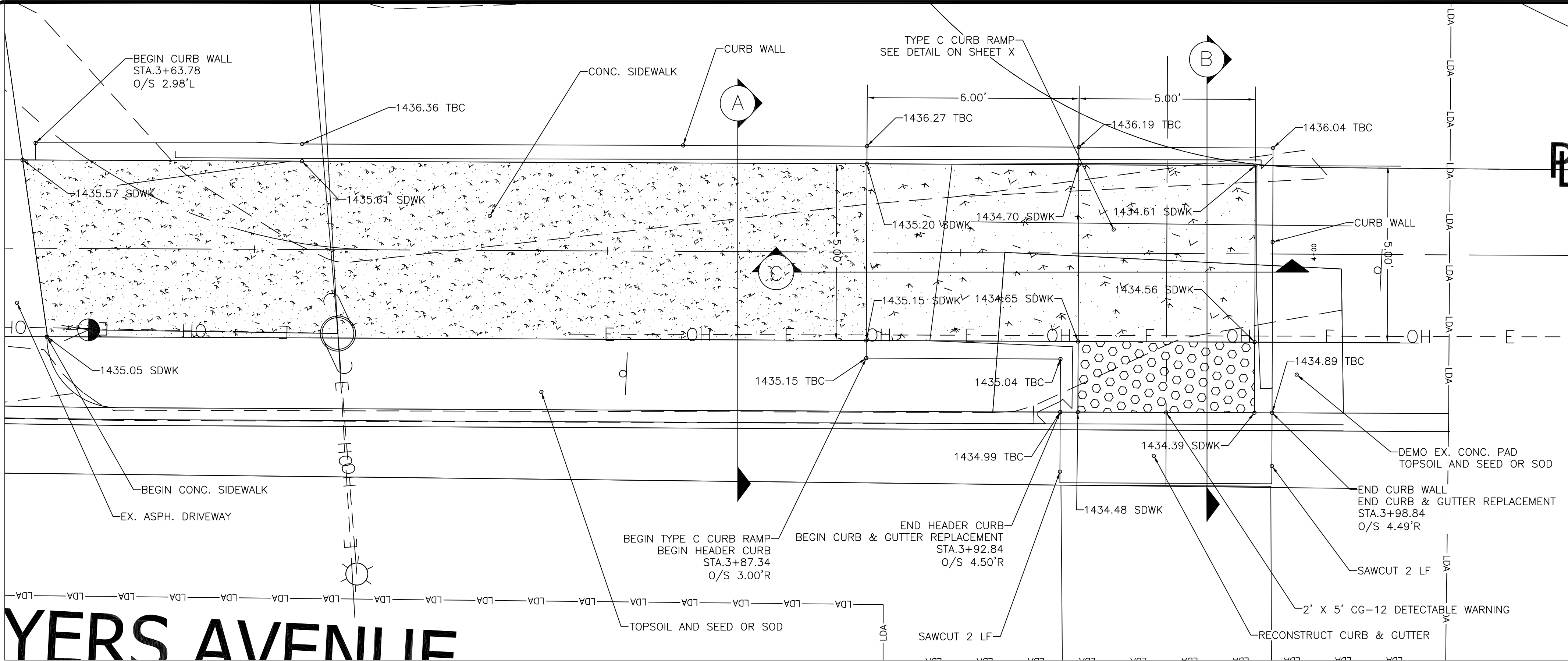


Mountain View Drive Trail

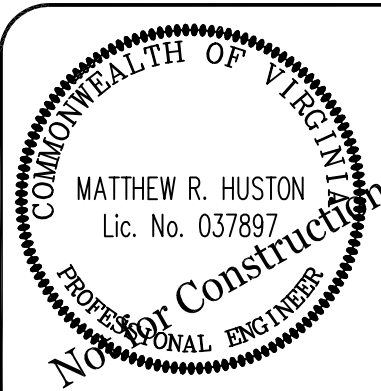
Plan and Profile Sta 2+00 to 4+00

Engineer:	MRH	Scale:	1" = 10'	Sheet Number: 9 of 14
Drawn By:	MRH	Date:	February 2021	
Checked By:		Tax Map No:	026, 027	

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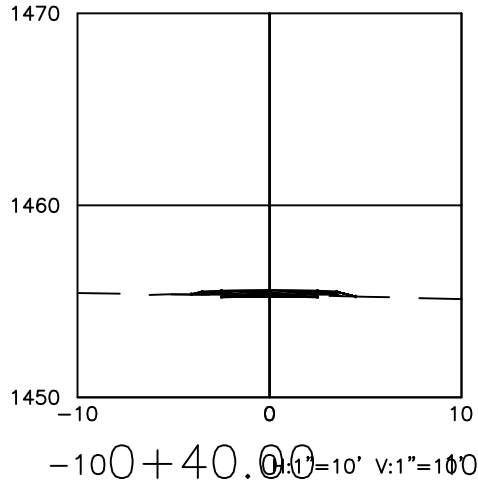
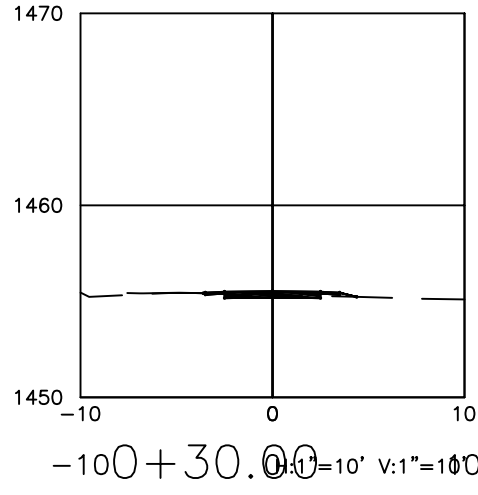
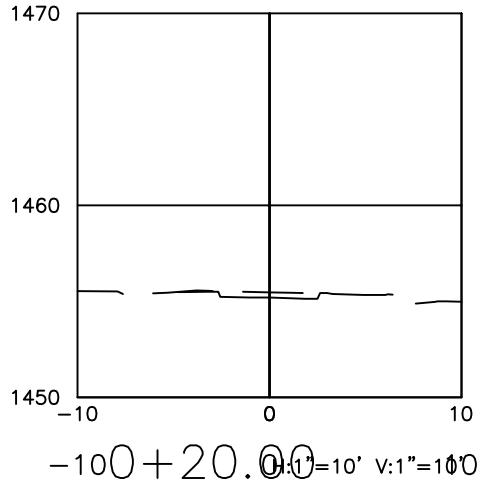
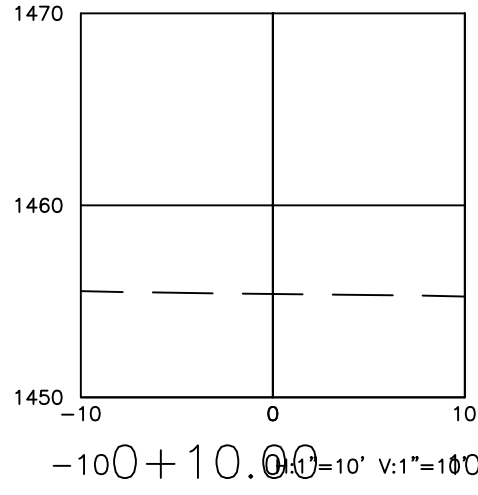
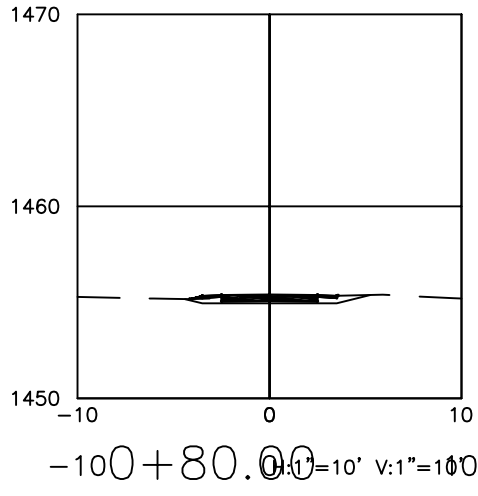
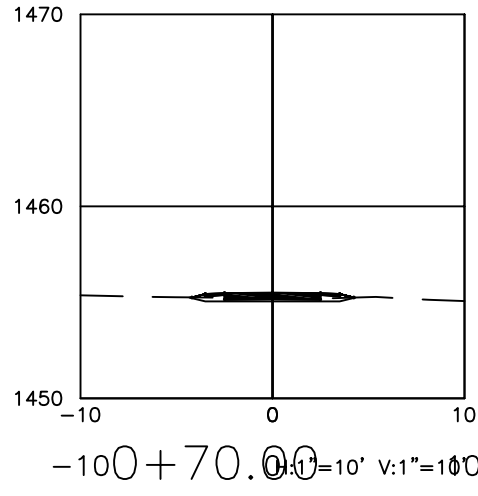
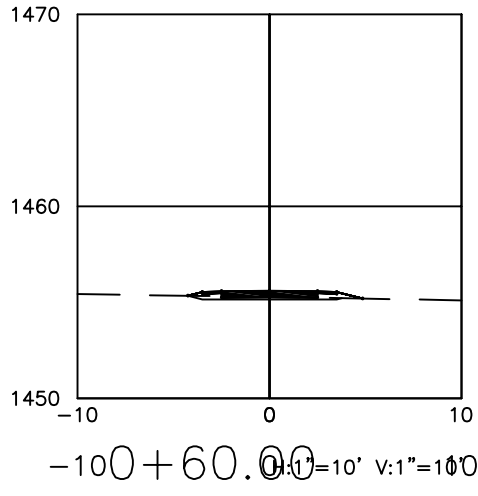
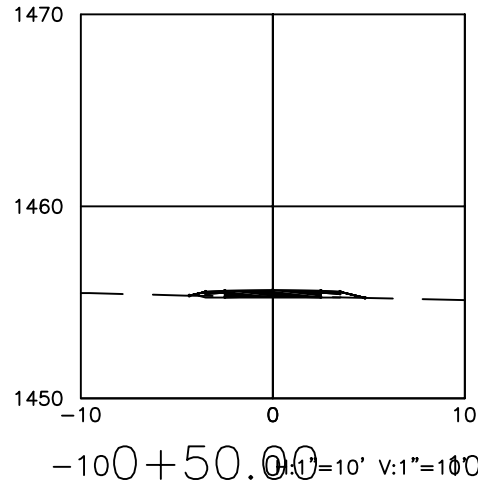
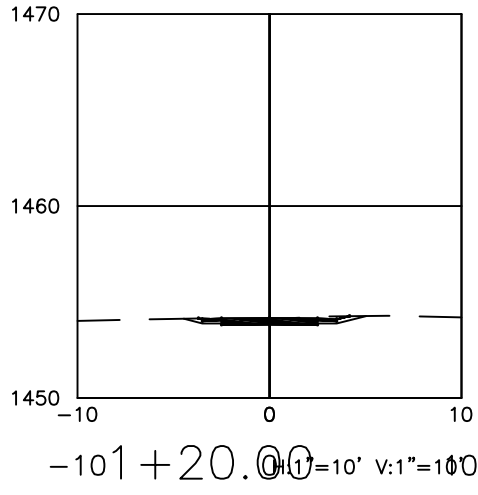
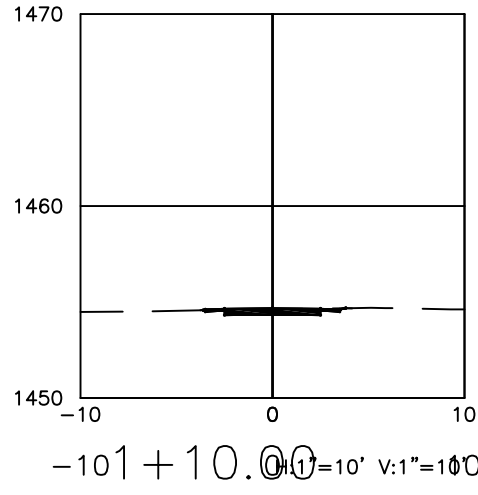
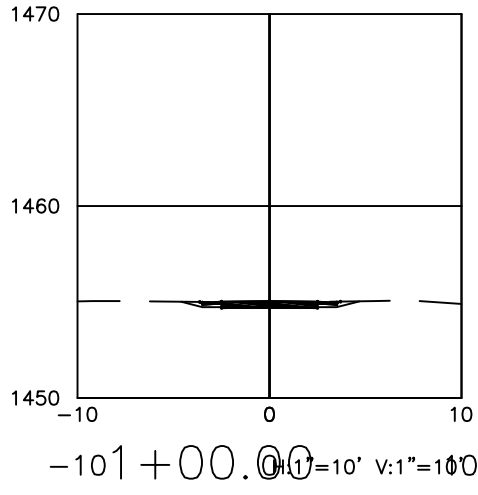
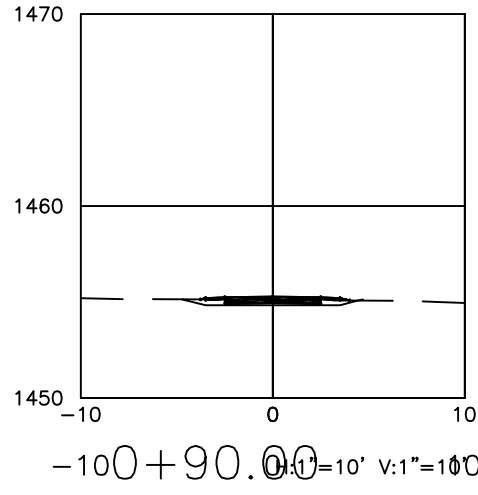


Mountain View Drive Trail
Curb Ramp Details

Engineer:	MRH	Scale:	1" = 2'
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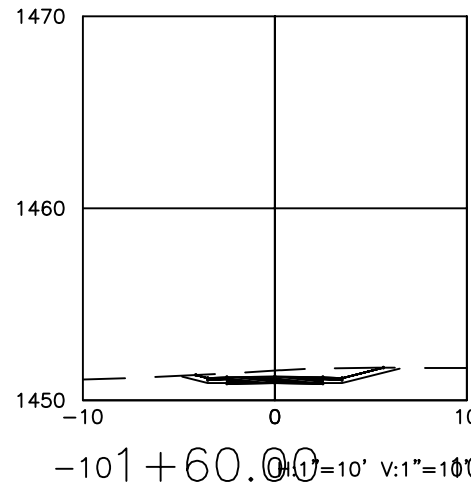
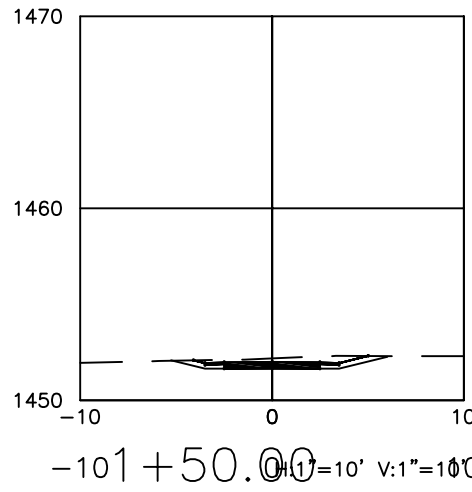
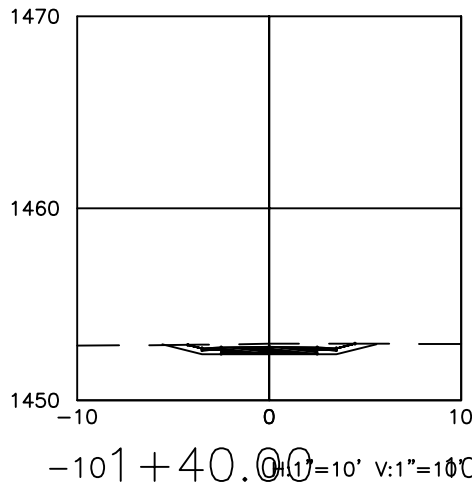
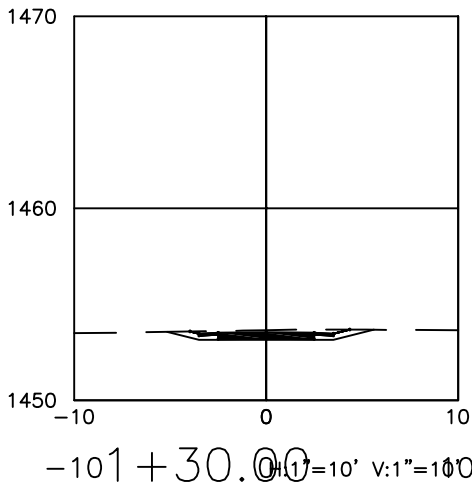
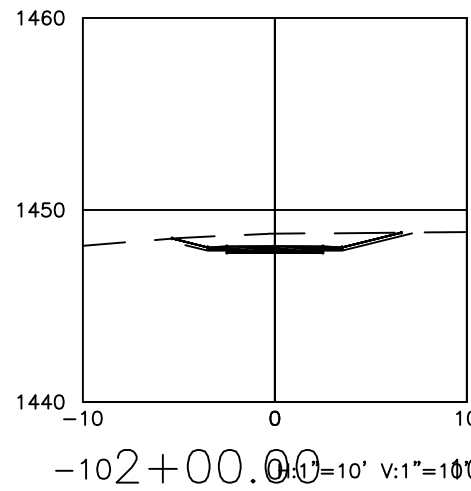
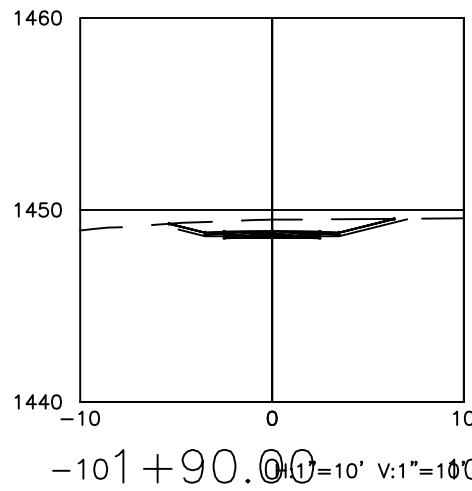
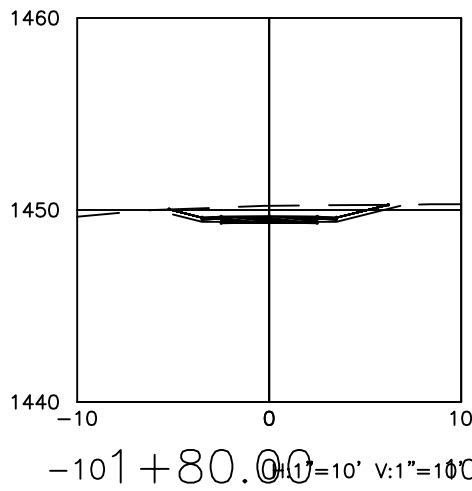
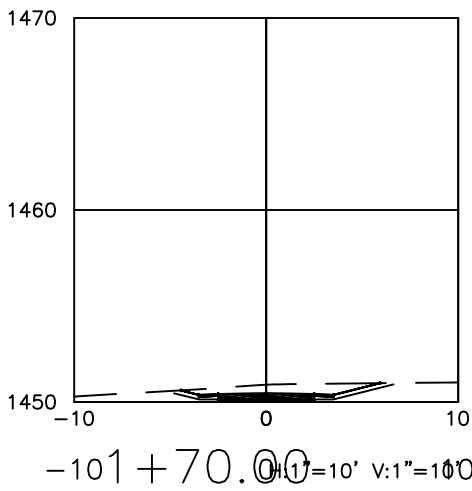
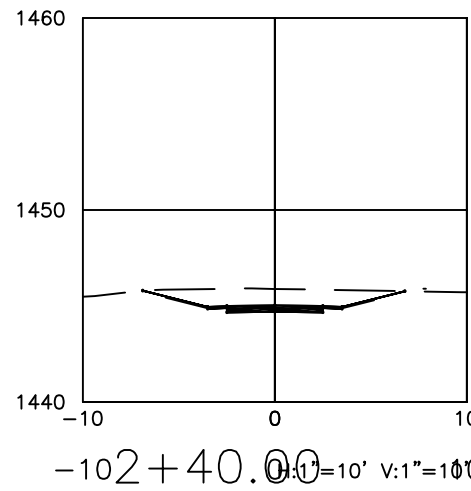
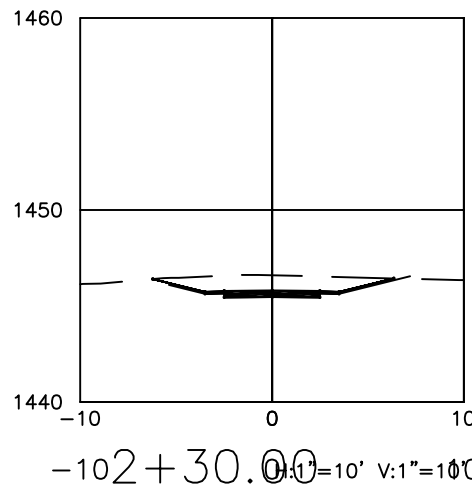
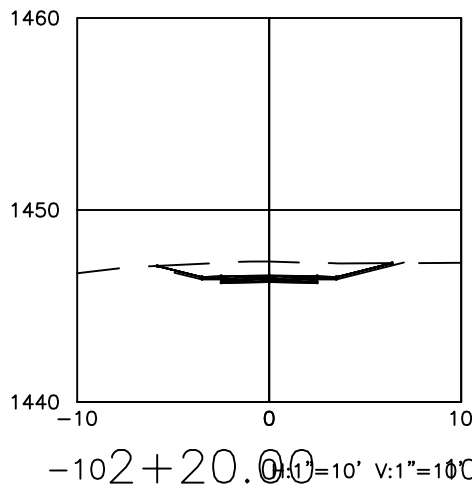
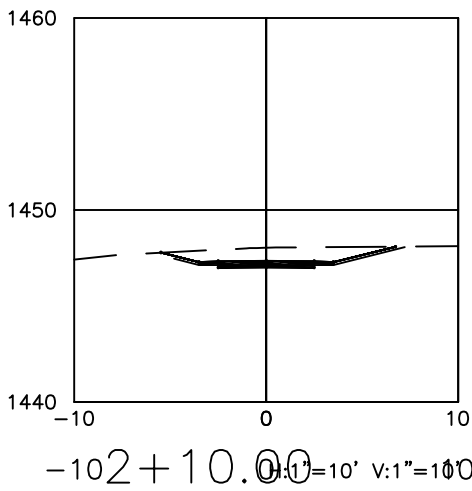


Mountain View Drive Trail
Cross Sections Sta 0+10 to 1+20

Engineer:	MRH	Scale:	1" = 10'
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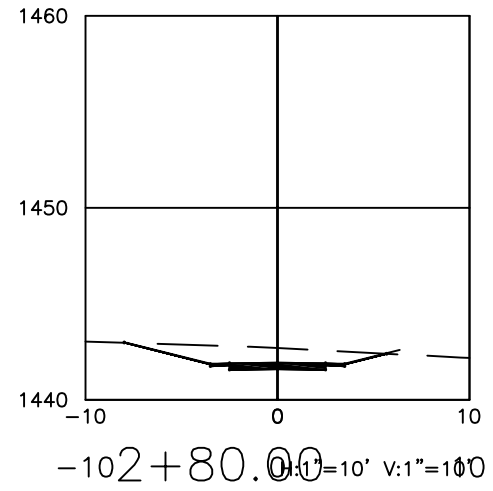
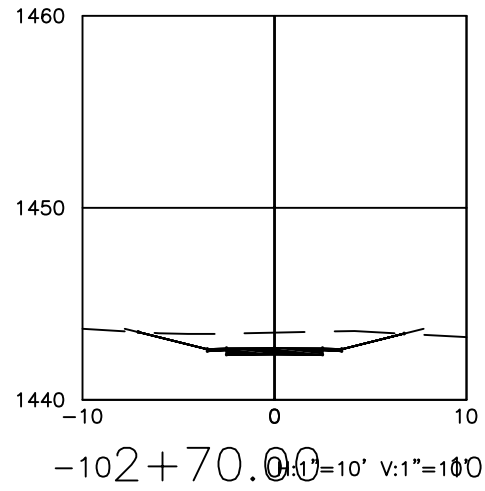
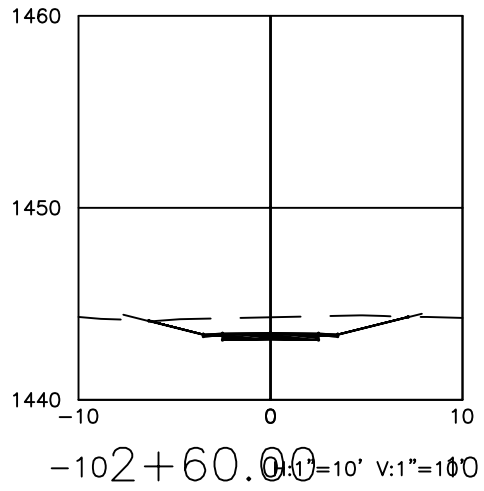
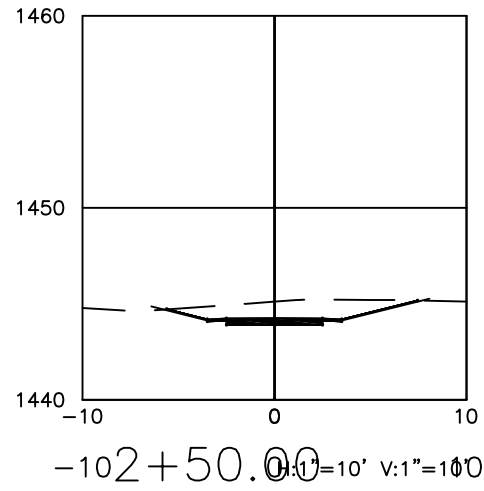
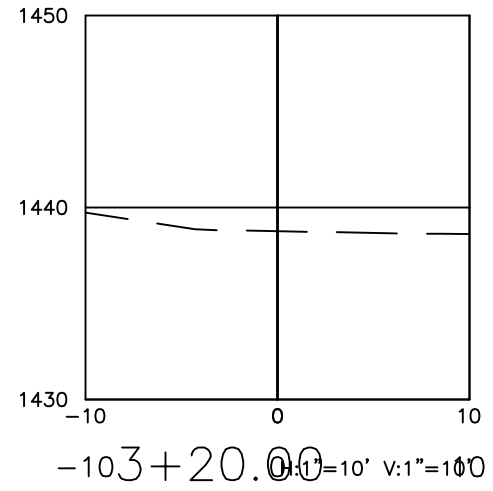
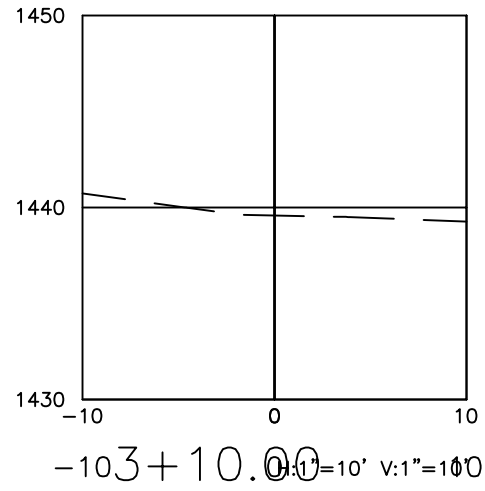
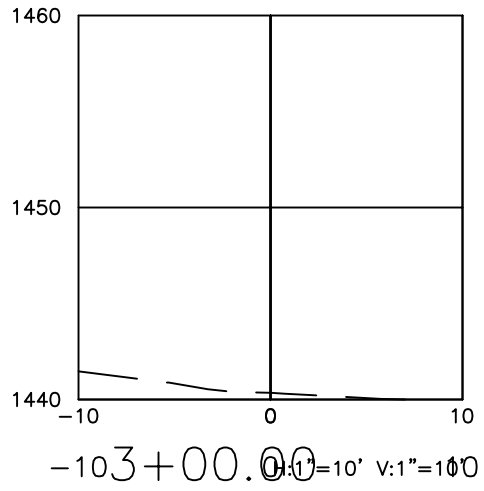
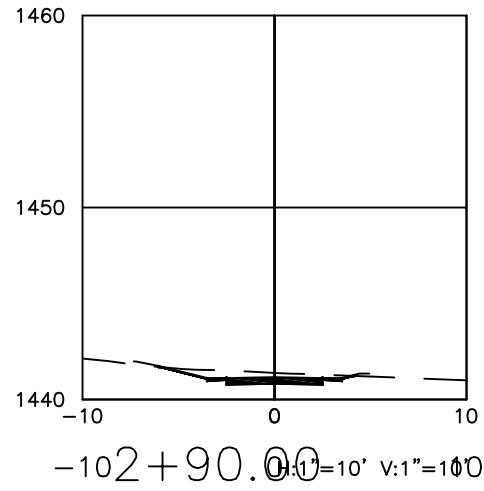
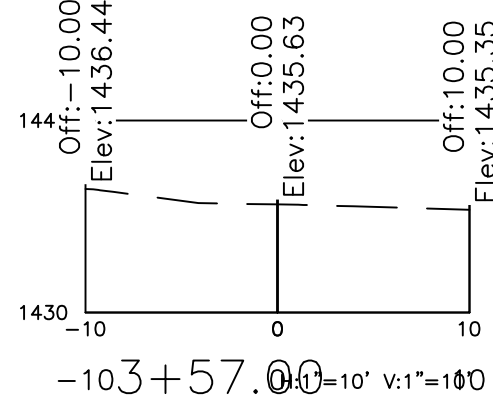
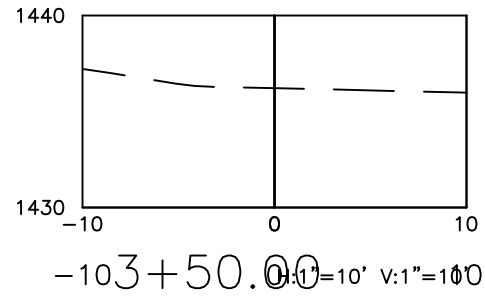
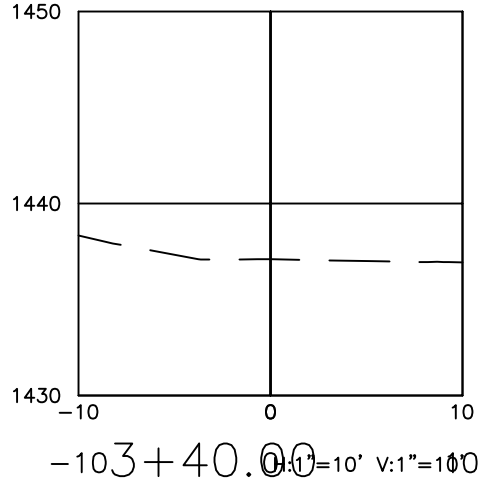
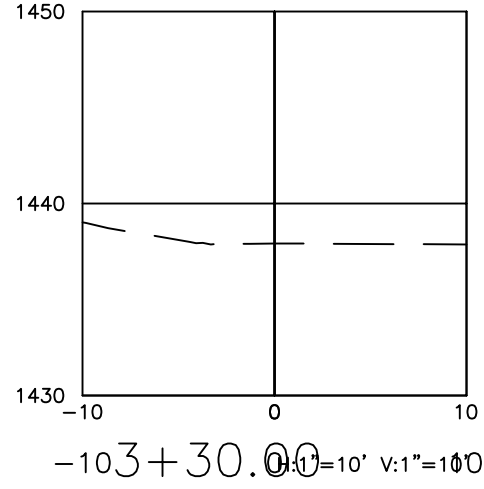


Mountain View Drive Trail
Cross Sections Sta 1+30 to 2+40

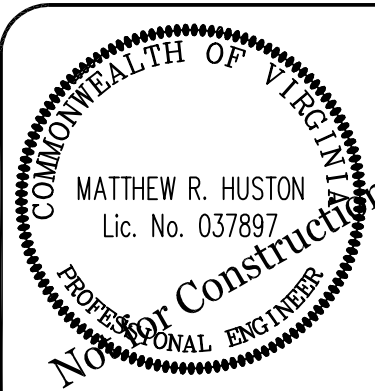
Engineer:	MRH	Scale:	1" = 10'
Drawn By:	MRH	Date:	February 2021
Checked By:		Tax Map No:	026, 027

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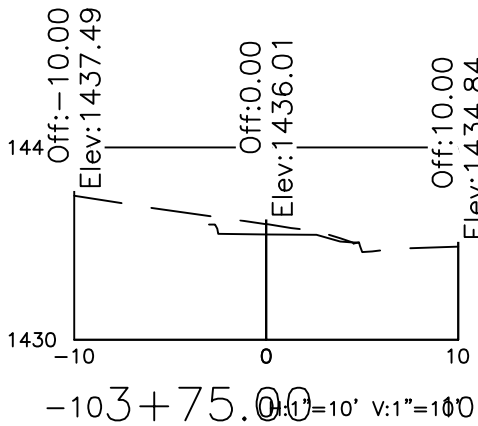
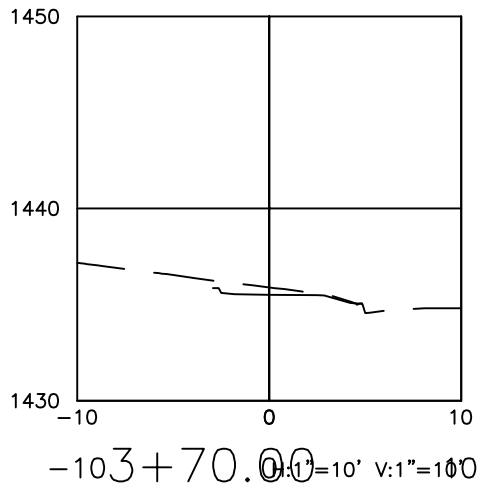
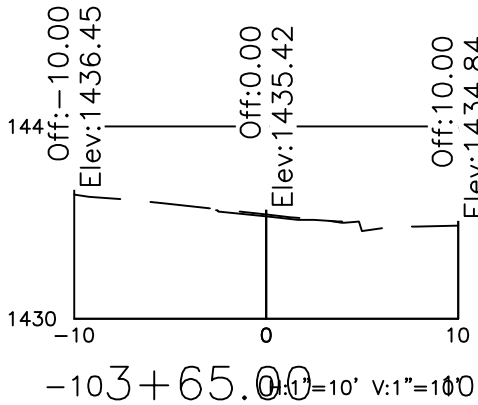
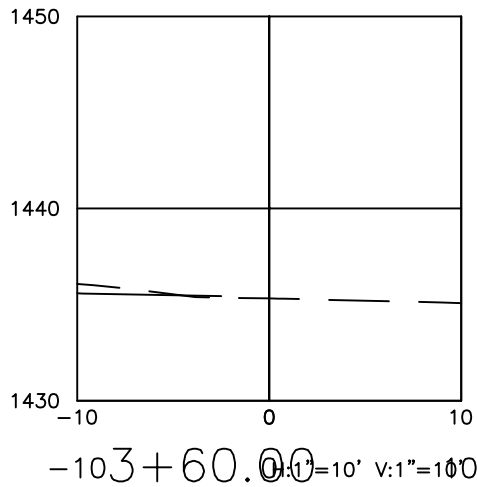
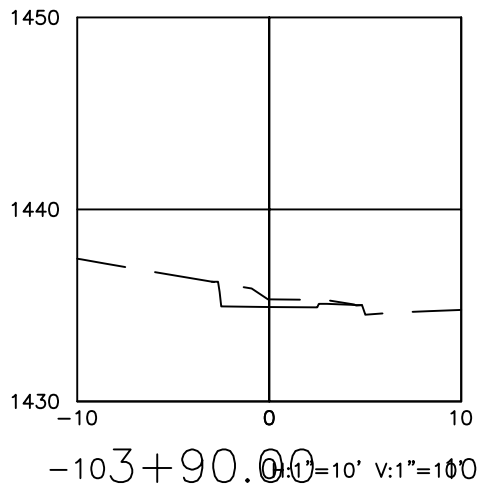
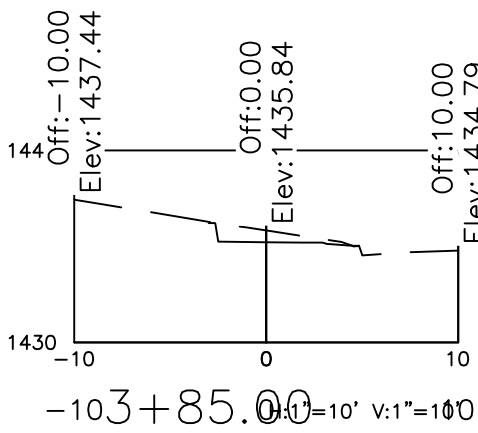
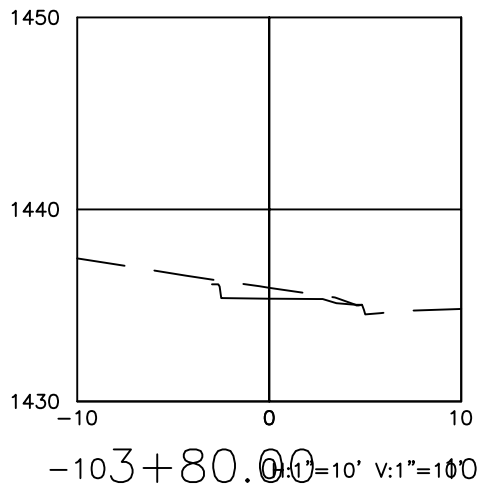
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Mountain View Drive Trail
Cross Sections Sta 2+50 to 3+58

Engineer:	MRH	Scale:	1" = 10'
Drawn By:	MRH	Date:	February 2021
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Mountain View Drive Trail
Cross Sections Sta 3+60 to 3+90

Engineer:	MRH	Scale:	1" = 10'
Drawn By:	MRH	Date:	February 2021
Checked By:		Tax Map No:	026, 027